



## **BUCKLEY CITY COUNCIL STUDY SESSION AGENDA**

**August 2, 2016  
Multi-Purpose Center, 811 Main Street  
City Council Meeting  
Opening 7:00 P.M.**

### **Agenda**

1. AHBL Presentation: Design Standards
2. Miscellaneous Discussion



## City of Buckley

P.O. Box 1960 • Buckley, WA 98321 • (360) 829-1921 ext. 7801

To: Mayor Pat Johnson  
Members of the Buckley City Council  
From: Planning Commission  
Date: July 12, 2016  
Re: Design Guidelines

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### Documents.

1. Gateway Subarea Plan.
2. The revised Design Guidelines.
3. Associated maps of the three design areas.
4. Revision to BMC 19.51.

### Background.

In 2015, the city council transmitted the Draft Rainier Gateway Subarea Plan and design guidelines. The planning commission reviewed the draft Gateway subarea plan, the proposed Gateway design standards, and in addition, reviewed the existing historic design guidelines and the SR 410 design guidelines.

The resulting study reorganized the existing standards, renamed them “guidelines,” which is a more accurate term, and added the Gateway standards. The commission also worked on combining Chapters 19.50 and 19.51, but the revision is still being considered by the city attorney.

### Recommendation.

The planning commission recommends approval of the documents with the following additional recommendations. The planning commission would like the city council to re-assess the following issues:

1. The location of the trail crossing. The planning commission would like the trail crossing to be located near its current location crossing SR 165 , and if necessary, install a flashing crosswalk sign similar to the signal at Mason and SR 410. The current location, which on Figure 10 of the subarea plan, would be between the two proposed buildings. The commission thinks locating the trail between these two buildings would be advantageous to the future businesses that would locate there. Alternatively, the trail should be relocated to the intersection of SR 165 and 112<sup>th</sup> St E.
2. The maps in the subarea plan need to be updated, particularly Figure 4, zoning in the SR 410 subarea.
3. The planning commission recommends a revision to the historic design area, which should be reflected in the subarea plan.

**ORDINANCE NO. 16 - \_\_\_\_\_**

**AN ORDINANCE OF THE CITY OF BUCKLEY, WASHINGTON, CONCERNING DESIGN GUIDELINES IN COMMERCIAL DISTRICTS, AMENDING CHAPTER 19.51, BMC; AND ADOPTING DESIGN GUIDELINES; PROVIDING FOR SEVERABILITY; AND ESTABLISHING AN EFFECTIVE DATE.**

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WHEREAS, the city created the City of Buckley Guidelines for Redevelopment in 1991 and updated the guidelines under Ordinance 11-08 in 2008; and

WHEREAS, the city is reshaping its highway corridor through a construction plan; and

WHEREAS, the highway corridor is the city's "front yard"; and

WHEREAS, the highway corridor between SR 410 and River Avenue is largely owned by the city and zoned for commercial development; and

WHEREAS, the area between SR 410 and River Avenue contains the Foothills Trail and should meet a commercial design that welcomes the public into the city; and

WHEREAS, the city council reviewed a subarea plan for the Rainier Gateway and transmitted the draft plan to the planning commission for processing; and

WHEREAS, the planning commission reviewed the existing design standards to update and clarify; and

WHEREAS, the planning commission conducted a public hearing on this proposal on June 6, 2016; and

WHEREAS, the required notice was received July 5, 2016, by the Washington State Department of Commerce under Material Identification Number (MID#) 22591 informing it of the proposed change in development regulations; and

WHEREAS, environmental review was performed on the proposal and a determination of non-significance was issued July 5, 2016;

NOW, THEREFORE,

THE CITY COUNCIL OF THE CITY OF BUCKLEY, WASHINGTON, DO ORDAIN AS FOLLOWS:

Section 1. The City of Buckley Guidelines for Redevelopment is hereby repealed.

Section 2. The City of Buckley Rainier Gateway Subarea Plan attached as Attachment A is hereby adopted in full.

Section 3. The City of Buckley Design Guidelines attached as Attachment B together with its attached three maps of the commercial district areas are hereby adopted in full.

Section 4. A new section is added to Chapter 19.51 BMC as follows:

**19.51.080 Rainier Gateway District.**

- (1) The purpose of the gateway district is to:
  - (a) The design of the Rainier Gateway Subarea shall reflect the community's desire to preserve open space and views of Mount Rainier.
  - (b) Transition from the auto-dependent SR 410 to the more pedestrian-friendly historic Downtown
  - (c) Strive to build non-motorized connections created by the Foothills Trail.
- (2) The design review requirements of this chapter pertain to all structures and fixtures, except single-family detached residences, and to all residential structures that are used for commercial purposes, if such structures lie within the Gateway design review district.
- (3) The area of the Rainier Gateway District is roughly described as that area between SR 410 to the northwest and River Road to the southeast, SR167/Ryan Road to the southwest, and Park Avenue to the northeast.

Section 5. Copy to the Department of Commerce. Pursuant to RCW 36.70A.106, the City Administrator is hereby authorized and directed to provide a copy of this ordinance to the State Department of Commerce within 10 days of adoption.

Section 6. Severability. If any section, sentence, clause or phrase of this ordinance should be held to be invalid or unconstitutional by a court of competent jurisdiction, such invalidity or unconstitutionality shall not affect the validity or constitutionality of any other section, sentence, clause or phrase of this ordinance.

Section 7. Effective date. This ordinance or a summary thereof consisting of the title shall be published in the official newspaper of the city, and shall take effect and be in full force five (5) days after publication.

APPROVED by the Buckley City Council this  
day of  
, 2016.

\_\_\_\_\_  
MAYOR, PAT JOHNSON

ATTEST/AUTHENTICATED:

\_\_\_\_\_  
CITY CLERK, JOANNE STARR

APPROVED AS TO FORM

OFFICE OF THE CITY ATTORNEY:  
BY \_\_\_\_\_

PUBLISHED:

PHIL OLBRECHTS \_\_\_\_\_ EFFECTIVE:

**CITY OF BUCKLEY**  
**DRAFT RAINIER GATEWAY SUBAREA PLAN**

JANUARY 18, 2016

PREPARED FOR THE CITY OF BUCKLEY, WASHINGTON



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## 1 INTRODUCTION

### 1.1 OVERVIEW

Buckley, Washington, is a small City located in the south Puget Sound, thirty miles east of Tacoma and near the foothills of Mt. Rainier. Buckley is facing the challenge of trying to preserve its small-town character while accommodating economic and population growth. The community is concerned about the sprawling physical expansion and intensive commercial development that has happened in nearby cities to the west and north occurring in Buckley.

As a result, in 2002 the City Council established the goal of creating a Subarea Plan for the State Route (SR) 410 railroad right-of-way corridor. The SR 410 railroad right-of-way corridor is bounded SR 410 to the northwest, River Avenue to the southeast, SR 165/Ryan Road to the southwest, and Park Avenue to the northeast (see Figure 1). In this document, it is referred to as the Rainier Gateway Subarea Plan area.

In 2003, the National Park Service, in cooperation with the U.S. Forest Service and local jurisdictions located in the Carbon River Corridor, including Buckley, conducted a charrette to envision growth in the corridor related to recreational activities. Many of the goals and design themes developed in this charrette became part of the City's 2005 Comprehensive Plan update, conducted by the City with the assistance of students from the University of Washington's Master of Urban Planning program.

The City's 2005 Comprehensive Plan identified recreational aspects of the community as being a key to promoting economic development and developed recommendations for redeveloping the property within the Rainier Gateway Subarea. The availability of vacant land within the City-owned former railroad right-of-way, in conjunction with its proximity to the Foothills Trail, SR 410, and the downtown business core, make it an ideal location for development that will support the City's vision. The intention of the Rainier Gateway Subarea Plan is to consolidate and adopt the planning work done by the City over the last ten years as formal policies and regulations.

**Comment [BM1]: AHBL Comment:** Need to incorporate 2015 Comprehensive Plan update materials into the final draft of the Rainier Gateway Subarea Plan.

### 1.2 CONTEXT

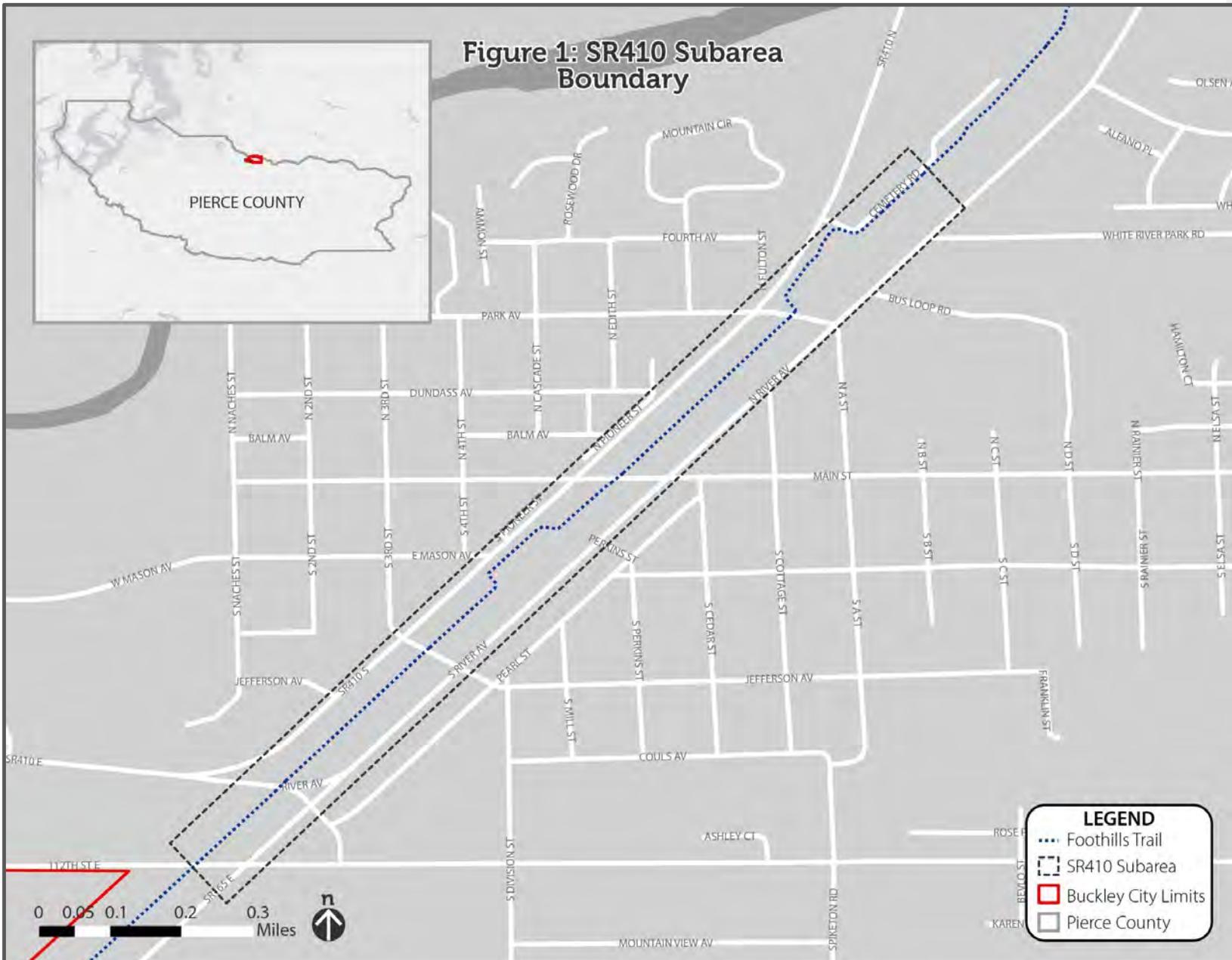
The City of Buckley is approximately four square miles in area and sits on the White River plateau next to the southern Cascades. Two state highways intersect in Buckley. SR 410 connects Sumner to Yakima and SR 165 links Buckley to Wilkeson, Carbonado, and Mt. Rainier's Carbon Glacier, as well as connecting to SR 162, which leads to South Prairie and Orting. The City and the state are currently working on the Ryan Road realignment project, which will improve the intersection of SR 410 and SR 165, as well as redefine the southern boundary of the Rainier Gateway Subarea Plan. Plans also exist to connect Buckley's Foothills Trail to Enumclaw and King County, with a bridge over the White River. The Washington State Department of Transportation (WSDOT) is currently replacing the steel-truss Meridian Street Bridge in Puyallup and has identified the potential to preserve the old bridge for use on the Foothills Trail between Enumclaw and Buckley across the White River.

The City of Buckley incorporated in 1889, although settlers had been arriving in the area since 1830, beginning with fur trappers and then miners after the discovery of coal in the upper Carbon River area in the late 1860s. Logging became the primary trade in Buckley; however, because of the decline in the logging industry, the City has seen limited population and job growth.

The City was skirted by the Wilkeson rail line, built in 1877 between Tacoma and Wilkeson, but then became a flag station on the Northern Pacific Railroads first transcontinental rail line in 1884. In 1982, the Burlington Northern Railway ended service on the tracks, and the railroad bed has since been acquired by the City and Pierce County and converted into the Foothills Trail. The 25-mile-long trail has become a popular recreational destination for bicyclists. The trail currently consists of 15 miles of paved non-motorized trail from South Puyallup to South Prairie and a two mile paved section in Buckley. Plans exist to connect the Foothills Trail in Buckley to the main paved trail ending in South Prairie in 2015, as well as continue to travel north through the City to King County and Enumclaw.

PC HEARING draft 20160606

**Figure 1: SR410 Subarea Boundary**



The City of Buckley has a population of 4,430 people, according to the Office of Financial Management’s 2014 estimate. The City’s population is projected to grow to 7,888 people by the year 2035, based on the population allocation target assigned to the City by Pierce County for 2030 and the City’s growth targets identified in the 2015 Comprehensive Plan.

While the Rainier School is the largest employer in the City, employing approximately 900 people, many citizens are employed outside of the City, due to a lack of other local employers. Within the City, social, educational, and health services make up the largest employment base, which is approximately 21 percent of the total base. The City has shortages of jobs in retail, manufacturing, and financial services, and continues to lose both retail sales and retail properties to neighboring jurisdictions.

Buckley has much to offer in the way of outdoor recreation, including hiking, cycling, fishing, kayaking, skiing, snowshoeing, and bird and wildlife viewing. The Foothills Trail is dubbed locally as “the new Main Street” because it serves as a major thoroughfare for pedestrians and bicycle traffic. It is a place where the community gathers for events and for recreation, and is a center of local importance.

### 1.3 TIMELINE/ RAINIER GATEWAY SUBAREA PLANNING PROCESS

The Rainier Gateway Subarea Plan is the culmination of strategic decisions and planning work completed over the last 13 years. In their 2002 visioning process, the Buckley City Council established the goal of completing a Master Development Plan for the Railroad Property. In 2003, the City, along with several other plateau communities, received a grant from the National Park Service to conduct several public visioning charrettes and create a plan for envisioning growth along recreational themes in the Carbon River Corridor. The charrettes resulted in general goals and design themes for the City, which formed the basis for the Comprehensive Plan updated in 2005.

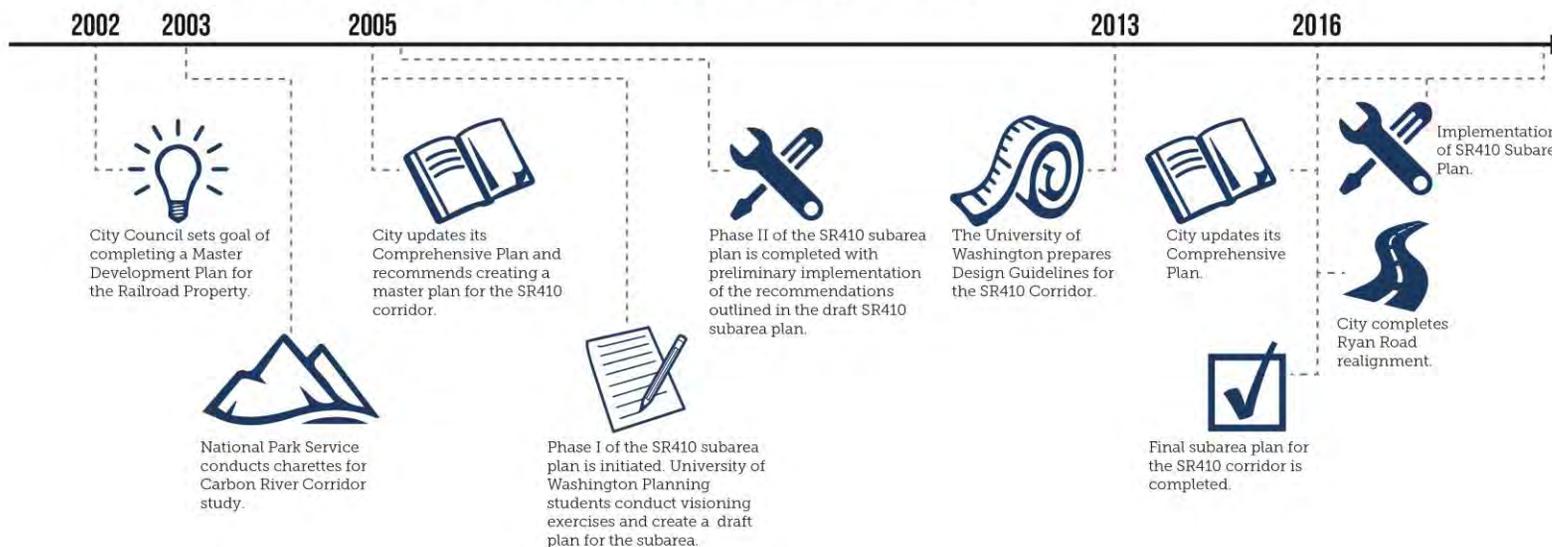
The University of Washington’s Master of Urban Planning class assisted the City in updating its Comprehensive Plan in 2005. As part of the update, staff and students conducted various public visioning workshops where they received ideas and suggestions for the future of the City, which ultimately formed the basis for the goals and recommendations added to the Comprehensive Plan.

One of the goals of the 2005 Comprehensive Plan was to plan for the development of the vacant property along the Rainier Gateway Subarea so that it would draw people to the downtown business area. The Comprehensive Plan identified recreational aspects of the community and surrounding area as key to attracting and promoting economic development within the City. With its availability of vacant land and proximity to the Foothills Trail and downtown business core, the City identified the Rainier Gateway Subarea as an ideal area for an overlay zone for development that would be consistent with the City’s vision.

After the completion of the Comprehensive Plan, the first phase of planning for the Rainier Gateway Subarea was initiated. The University of Washington’s Master of Urban Planning class assisted the City in creating the first draft of the Rainier Gateway Subarea Plan, and developing an implementation plan for phase two of the planning process. The preliminary implementation was completed in 2005 using a \$10,000 grant from CTED. Full implementation of the overlay performance standards was delayed until the Ryan Road realignment project and a zoning code update had been completed.

In 2013, the City contracted the University of Washington’s Department of Urban Design & Planning to prepare Design Guidelines for the Rainier Gateway Subarea. The students from the program gathered data, researched best practices, and engaged with the citizens of Buckley through public meetings to form an understanding of the City’s

Figure 2: City of Buckley SR410 Subarea Planning Timeline



economic conditions and community goals. The students then prepared recommendations for the City and ultimately presented Design Guidelines for the Rainier Gateway Subarea to the City Council.

In 2015, as part of its 2015 Comprehensive Plan update process, the City entered its final phase of planning for the Rainier Gateway Subarea. The final Rainier Gateway Subarea Plan includes findings from the previous planning work done for the Rainier Gateway Subarea and incorporates policies and regulations that will govern development within the Rainier Gateway Subarea.

After the adoption of the Rainier Gateway Subarea Plan is complete, implementation of the recommendations in the plan will result in realizing the City's vision for the Rainier Gateway Subarea. The City will be the lead implementer on most of the policies and recommendations, with the assistance of public/private partnerships, and community groups.

#### 1.4 KEY ISSUES

Commercial activity in Buckley has suffered from competition in neighboring communities and economic recession, reducing the desire of outside investors and local entrepreneurs to invest in Buckley. Because of underinvestment, local demand for everyday goods and services has not been met, forcing residents to leave the City to work and shop. Buckley lacks certain commercial offerings, but it has a collection of historical, character-rich storefronts along the Main Street.

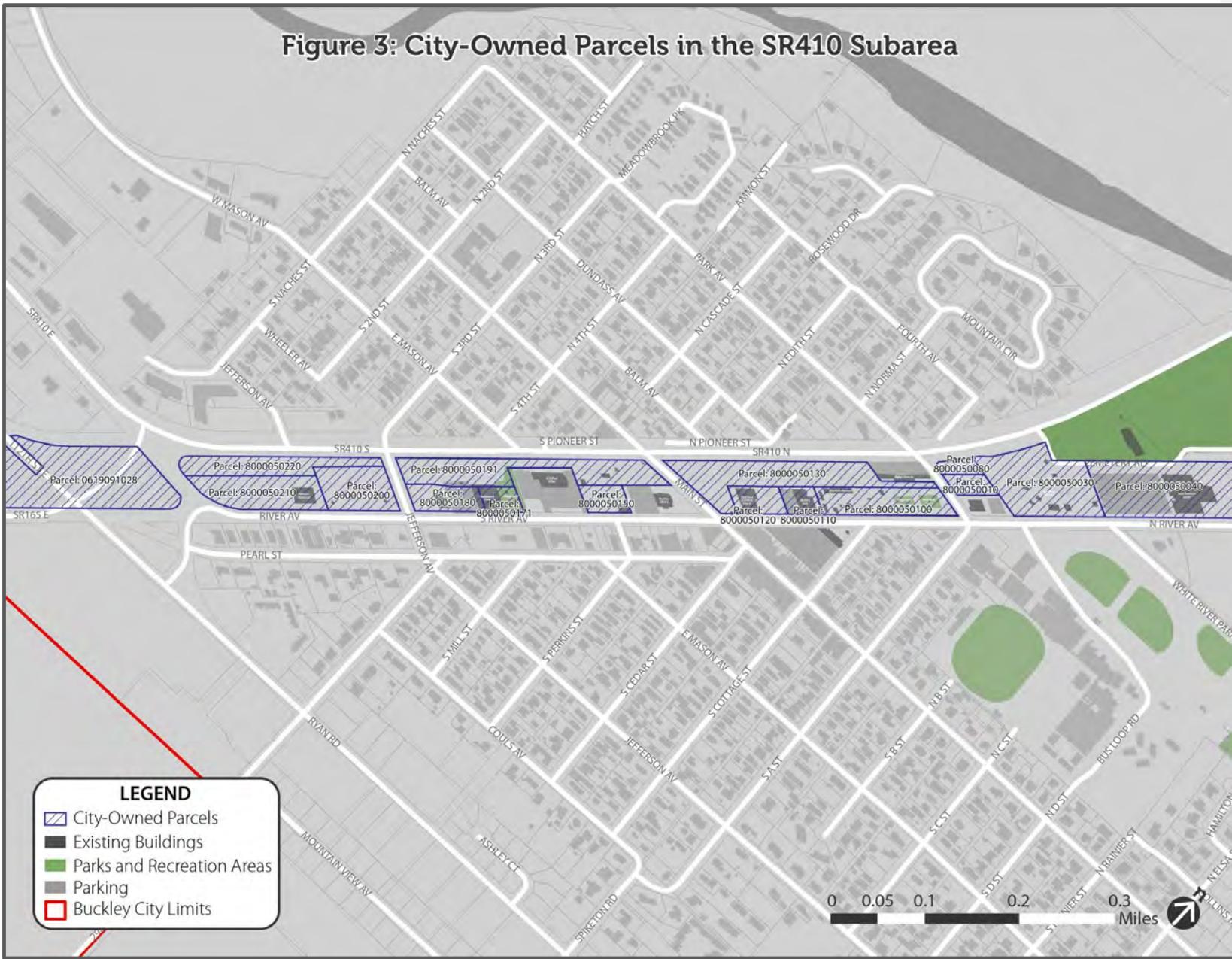
Buckley is interested in accommodating anticipated local growth in a manner that supports Buckley's character. The City anticipates that new investment into Buckley will occur in the form of new residential and retail uses within the next few years. The City understands growth is needed, but that growth should be reasonably controlled so that the City's valuable assets are preserved for current and future residents.

The City of Buckley owns multiple parcels within the Rainier Gateway Subarea that may be of interest to potential investors (see Figure 3). Buckley has the opportunity to shape development within the Rainier Gateway Subarea by dictating the form and intent of each parcel through design guidelines. The sale of key City-owned parcels should be phased strategically in order to link the Rainier Gateway Subarea to Buckley's historical downtown as the commercial and social center of the City.

Opportunities also exist in the Rainier Gateway Subarea to position the community as a "gateway to Mount Rainier" and a destination for tourists and residents alike, who are interested in outdoor recreational activities. Recreational uses should be encouraged within the Rainier Gateway Subarea, including expanding the Youth Center and Skateboard Park. Providing signs and amenities for Foothills Trail users will encourage them to stop in Buckley for food, shopping, and local events, and encourage them to return to Buckley.

The challenge facing Buckley is to create an economic climate that produces a healthy economy for jobs and businesses without compromising the community's desires to maintain its small town lifestyle and protect its natural amenities. The Rainier Gateway Subarea Plan aims to increase economic development in the City by establishing a business-friendly environment and attracting tourism by making Buckley a recreational destination with the presence of the Foothills Trail and proximity of Mt. Rainier.

**Figure 3: City-Owned Parcels in the SR410 Subarea**



## 2 EXISTING CONDITIONS

No major environmental or critical area constraints were identified that would affect development within the Rainier Gateway Subarea. The zoning designations in the subarea are Central Commercial (CC) and Public (P), as shown in Figure 4. The minimum lot size in the CC zone is 3,000 sf per unit and the minimum lot width is 40 feet. There are no minimum setbacks requirements for commercial uses, while commercial mixed-use development requires a 10-foot front, side, and rear setback. Setbacks in the Public zone are ten feet from commercially zoned properties and right-of-ways and in accordance with the building code for setbacks from other publicly zoned properties.

With respect to building massing, the maximum lot coverage excluding landscaping required for barrier or visual relief buffers in the CC zone for commercial mixed-use units is 70 percent, not including outbuildings or accessory units, and when they are included, the maximum lot coverage is 75 percent. The maximum lot coverage excluding landscaping required for barrier or visual relief buffers for commercial units is 100 percent. The P zone has no maximum requirements for lot area or lot coverage.

The maximum height in both the CC and P zones is 35 feet, which is effectively three stories, and off-street parking requirements vary by use. The surrounding area consists of High Density Residential (HDR), Low Density Residential (R-6,000), Historic Commercial (HC), and some Medium Density Residential (R-8,000).

### 2.1 LOCAL POLICIES & REGULATIONS

#### 2.1.1 COMPREHENSIVE PLAN

The Comprehensive Plan guides the City's future development based on the input of its citizens. The City of Buckley is committed to providing the components that contribute to the quality of life for residents and future generations. The key strengths of Buckley are its small town character and natural setting. However, Buckley faces challenges in the way of economic revitalization and population growth after the economic decline of late. In addressing these challenges, the City aims to preserve its small town character and natural environment.

The 2015 Comprehensive Plan update focused on three areas, which form the basis of the Plan's vision for the future. These areas were:

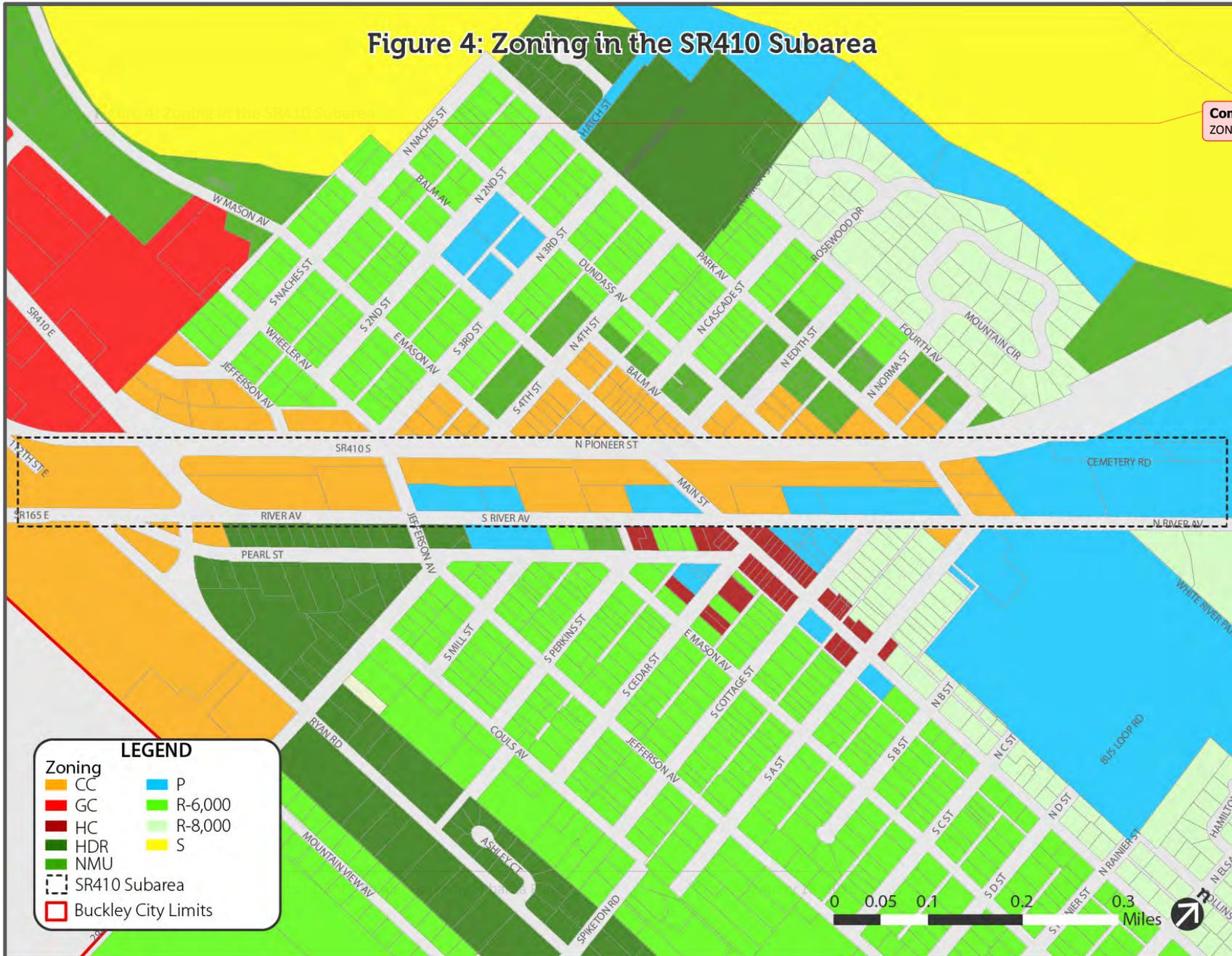
1. Preserve Buckley's town identity and character;
2. Develop the economic viability in the downtown core and SR 410; and
3. Promote outdoor recreational activities.

These focus areas established are the foundation for the goals for the City and the basis for the recommendations set forth in the Comprehensive Plan. These goals included creating an atmosphere that serves as a magnet for tourists, providing recreational activities for both residents and visitors, centering housing and commercial development in the downtown area, preserving the rural character outside of the downtown, and enhancing the small-town character of the City by creating a more pedestrian-oriented downtown.

The Rainier Gateway Subarea Plan must be consistent with the 2015 Comprehensive Plan, which included several policies to guide development in the Rainier Gateway Subarea.

Figure 4: Zoning in the SR410 Subarea

Comment [kj2]: NEED TO ADD UPDATED ZONING MAP



**LEGEND**

Zoning	
CC	P
GC	R-6,000
HC	R-8,000
HDR	S
NMU	
SR410 Subarea	
Buckley City Limits	

---

### 2.1.1.1 LAND USE ELEMENT

The land use element of the Comprehensive Plan set forth the following policies that support and guide the recommendations in the Rainier Gateway Subarea Plan:

---

**Policy 1.1.1**

*Buckley should preserve its small town character through the following:*

- 1. Concentrate retail development near the historic downtown center of the city and near the Rainier Gateway Subarea.*
- 2. Integrate additional density in the residential zones in a manner that protects the single-family areas from commercial encroachment.*
- 3. Focus commercial development outward from the existing commercial zones.*
- 4. Focus pedestrian and bicycling trails and sidewalks between commercial and residential developments to encourage non-motorized access.*
- 5. Encourage mixed-use developments in commercial zones.*

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**Policy 1.1.2**

*The city should formally designate the area between Ryan and Park, and between SR 410 and River Avenue the city's center of local importance (CoLI).*

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**Policy 1.6.4**

*Plazas and open space should be provided to link trails with commercial areas.*

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**Policy 1.8.7**

*New development shall provide community paths and trails that link the new development to existing trails and paths.*

---

### 2.1.1.2 HOUSING ELEMENT

The housing element of the Comprehensive Plan set forth the following policies that support and guide the recommendations in the Rainier Gateway Subarea Plan:

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**Policy 2.1.1**

*Incorporate neighborhood character and design principles into zoning and design review standards for new development.*

---

**Policy 2.1.3**

*Housing should be permitted above commercial uses in most commercial zones.*

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### 2.1.1.3 ECONOMIC DEVELOPMENT ELEMENT

The economic development element of the Comprehensive Plan set forth the following policies that support and guide the recommendations in the Rainier Gateway Subarea Plan:

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**Policy 3.1.2**

*The city's regulations should provide the following to enhance sustainable economic development:*

- a. Economic disincentives for vacant buildings.*
- b. Overlay district design standards for each commercial area, one for east/west-bound SR 410, one for the area adjacent to northeast-bound SR 410, and one for the historic district.*
- c. Procedures that are as streamlined as possible and still follow state requirements.*

---

**Policy 3.2.3**

*Historic downtown buildings should emphasize Buckley's small-town attributes through historic renovation.*

---

**Policy 3.4.3**

*The area between River Avenue and SR 410 is a place where people meet, recreate, and is an important open space area that gives the city a pleasant atmosphere. The area is zoned commercial and shops are planned to be erected between the Foothills Trail and River Avenue; the area between SR 410 and the trail will remain open space. The area should be designated with the county as a center of local importance and called the Rainier Gateway Center.*

---

**Policy 3.6.1**

*When possible, open space areas should be protected as natural areas for public enjoyment or developed into recreational facilities.*

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### 2.1.1.4 URBAN DESIGN ELEMENT

The urban design element of the Comprehensive Plan set forth the following policies that support and guide the recommendations in the Rainier Gateway Subarea Plan:

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**Policy 4.1.1**

*Develop pedestrian infrastructure by widening sidewalks, installing crosswalks, corner plazas, bollards, street furniture, and so forth.*

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**Policy 4.2.2**

*Develop a visual link between the trail and downtown by using elements such as pavement materials or landscaping. For example, the existing small plaza could be enhanced to help connect the trail with downtown.*

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**Policy 4.2.3**

*Develop a place of respite on the bike path as it passes Main Street to encourage trail users to stop and visit downtown.*

**Policy 4.2.4**

*Encourage retail consistent with recreational use between the trail and River Avenue.*

**Policy 4.4.6**

*Work toward a future regional transit stop in Buckley's downtown core through establishing public parking areas, park-n-ride lots, and commercial areas around public parking lots.*

**2.1.2 DESIGN GUIDELINES**

In 1991, the City of Buckley identified two key districts for redevelopment and adopted specific design guidelines: the Downtown Review District and the Highway Review District. Each district has specific guidelines pertaining to scale, detailing, setback requirements, sidewalk improvements, and details for renovation and construction of new and existing structures. The City reviews proposals for restoration and new development in the City's historic commercial core and the commercial and industrial corridor along State Route 410 in accordance with these guidelines.

The 1991 Design Guidelines for the Highway Review District guide the development of land along Highway 410 as it is converted from agricultural and low-density development to commercial and industrial uses. The design guidelines call for new development on SR 410 to be large in scale and flanked by large parking areas. Buildings should take inspiration from the barns and lumber mills built by the settlers and those who came after them on the Buckley plateau. Buildings and new development along SR 410 should be designed to reflect and reinforce the rural character of the area.

Currently, projects within the Rainier Gateway Subarea must comply with the Design Guidelines for the Highway Review District, including using traditional materials such as wood, stone, and metal, with long horizontal lines with sloping roofs. A 35-foot wide minimum landscaped buffer is required along the SR 410 frontage, with an irrigated planting strip greater than ten feet wide, a six-foot wide meandering sidewalk, and bermed parking areas. Within the Rainier Gateway Subarea long, uninterrupted buildings and developments are not acceptable. The Design Guidelines require either modulation or a variation of the building façade by shifting the elevation no less than four feet every 80 feet of building length. Individual buildings or developments within the Rainier Gateway Subarea are limited to more than 30,000 gross square feet in area. Buildings in the Rainier Gateway Subarea can be no more than two stories and have sloping roofs with a minimum pitch of 6:12.

**2.1.3 BMC § 19.51 HIGHWAY 410 DESIGN REVIEW DISTRICT**

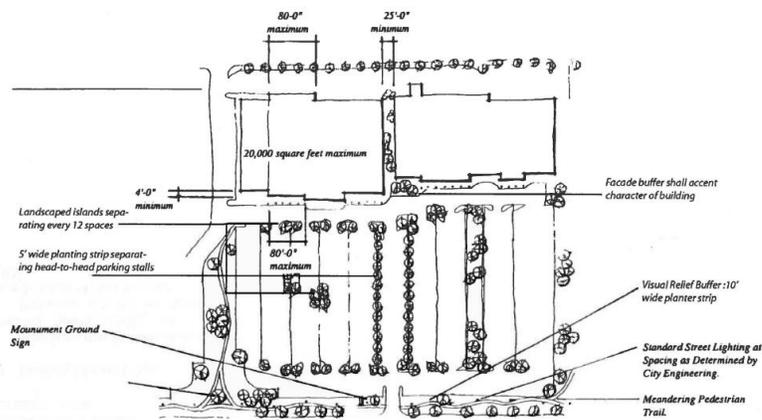
In 1991, to enforce the Design Guidelines adopted by the City of Buckley, regulate growth along SR 410, and prevent unsightly strip development, the City Council passed Ordinance 19-91 to create the Highway 410 Design Review District, [Chapter 19.51 of the Buckley Municipal Code](#) (BMC).

**Comment [BM3]:** AHBL Comment: The purpose (BMC 19.51.010), scope (BMC 19.51.030), description (BMC 19.51.040), and map of the Highway 410 design review district will need to be amended to remove the area subject to this subarea plan.

If the proposed development meets the following criteria, it is subject to review under the Highway 410 design review criteria:

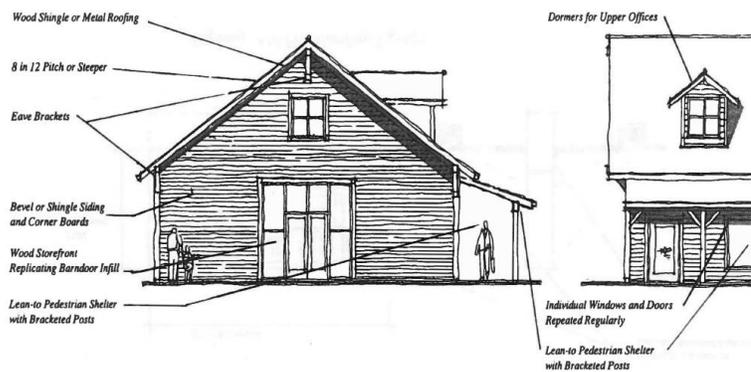
1. The fixture is not a sign which is otherwise regulated under BMC §19.30;
2. The structure or fixture is not a single-family detached residential dwelling, unless said detached single-family residential dwelling is used for commercial purposes;
3. The structure or fixture lies in the HC, GC, CC, LI, NMU, and P zone or is used for commercial purposes; and

**Figure 5: Example of a Commercial Site Development under the Highway Review Design Guidelines**



*Example: Commercial Site Development*

**Figure 6: Example of a Two Story Retail/Office Building under the Highway Review Design Guidelines**



*Example: Two Story Retail/Office*

4. The structure is proposed for property, which abuts SR 410 or is adjacent to properties that abut SR 410 and such property is also subject to review, and these properties abut one another on a common border or point.

For development proposed within the Highway 410 development area, and meeting the criteria above, the design review committee will review the application for construction or alteration. The design review committee will either (1) forward recommendations to the City Council for major construction or alteration that results in significant structural changes or total new construction or (2) vote to approve, deny or approve the application with modifications for minor construction or alterations that do not result in major structural changes.

## 2.2 COUNTY, STATE & FEDERAL POLICIES AND REGULATIONS

### 2.2.1 COUNTYWIDE PLANNING POLICIES AND VISION 2040 MULTI-COUNTY PLANNING POLICIES

The City is currently in the process of designating the Rainier Gateway Subarea a Center of Local Importance (CoLI). Under the Pierce County Countywide Planning Policies (CPP), areas that serve as important centers within Pierce County communities may be formally designated as a CoLI. The formal recognition of a CoLI would be used in future countywide project evaluations.

The CPPs establish design features for CoLIs that are consistent with the Puget Sound Regional Council's (PSRC) VISION 2040's Multi-County Planning Policies. A CoLI is characterized by a concentration of land uses or activities that provide a sense of place or gathering place for the community and neighborhood residents. A CoLI should include one or more of the following land uses:

- Civic services
- Commercial areas
- Recreational areas
- Industrial areas
- Cultural facilities/activities
- Historic buildings or sites
- Residential areas

A variety of transportation options and pedestrian friendly design should be available or planned within a CoLI. The City is responsible for defining what role the CoLI will play in supporting planned growth, and adopting the CoLI by documenting how the CoLI meets the design features specified in the Pierce County CPPs, and delineating the area on a map within the City's Comprehensive Plan. The City is then required to provide the Pierce County Regional Council (PCRC) notice of its intention to adopt a CoLI locally. The CoLI will then be recognized in Appendix B of the Pierce County CPPs.

### 2.2.2 NATIONAL PARK SERVICE – CARBON RIVER CORRIDOR

In 2003, the National Park Service collaborated with the U.S. Forest Service and local jurisdictions to complete a study on the Carbon River corridor on the north side of the Mount Rainier National Park, looking at portions of the SR 165, SR 162, and Rainier Gateway Subareas. The study included public outreach, with participation from the communities of Wilkeson, Orting, South Prairie, and Buckley, as well as the Puyallup Tribe.

The study explored a variety of opportunities to improve visitor experience in the corridor, and to strengthen the relationship between local communities and Mount Rainier National Park. Public workshops, stakeholder meetings, and interviews were held, followed by a charrette, an intense, idea generating design process to facilitate development of concepts and visualizations for potential activities within the corridor.

The charrette found most of the federal lands in the corridor are at or exceeding their capacity for recreational use. Opportunities for increased recreational capacity in the future will be found in the corridor communities and with the development of the Foothills Trail. There are also strong opportunities for the corridor communities to communicate their heritage to visitors through cultural facilities.

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### 2.2.3 STATE ENVIRONMENTAL POLICY ACT (SEPA) RULEMAKING 2014

In light of the increased environmental protections now in place under RCW 36.70A, RCW 90.58, and other laws and procedures for environmental protection, land use planning and infrastructure, in 2014 the Washington State Department of Ecology updated the State Environmental Policy Act (SEPA) Rules that guide state and local agencies in conducting SEPA reviews.

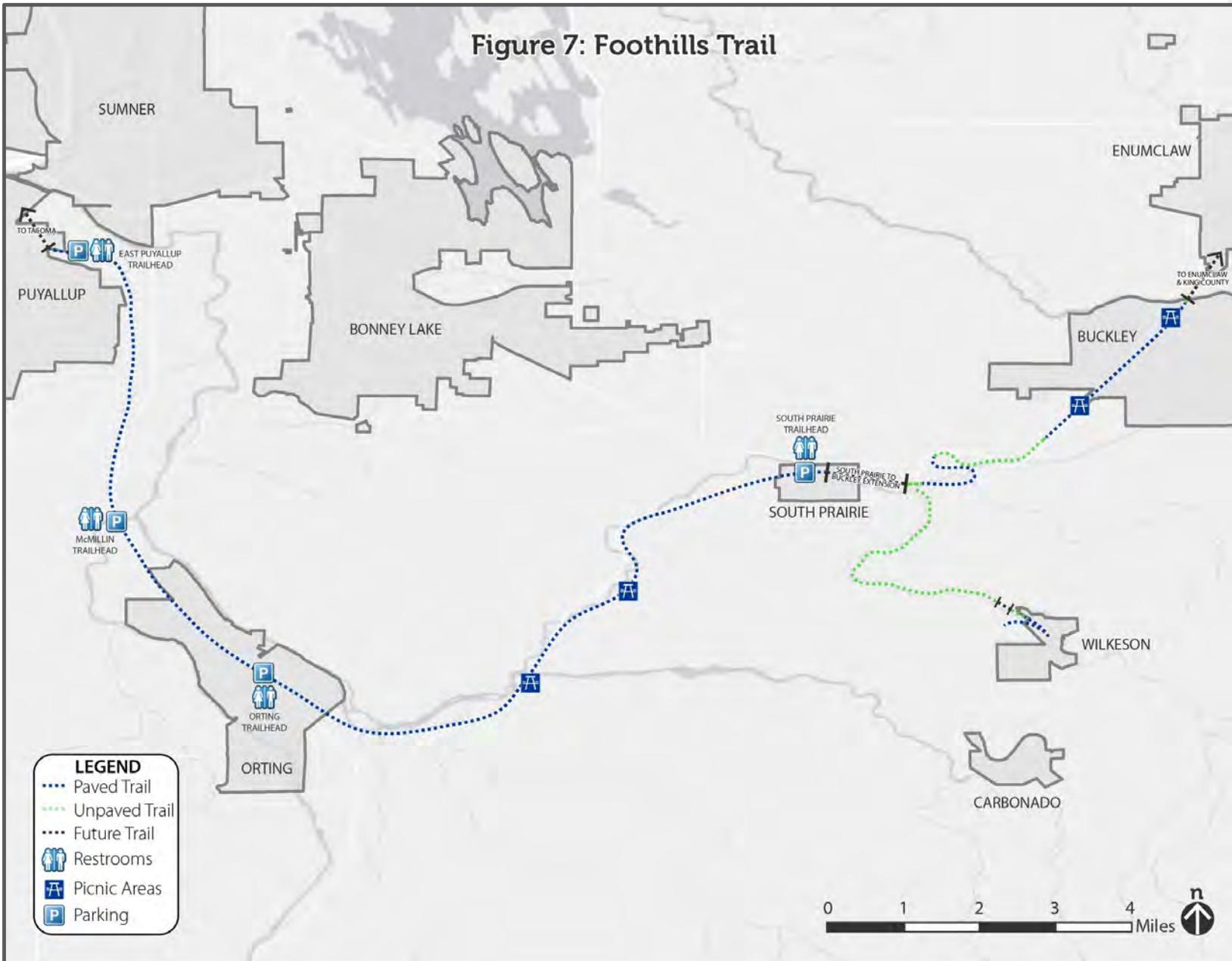
The updated SEPA rules (WAC 197-11-800(1)(d)) allow communities to adopt increased flexible exemption thresholds for minor new construction, provided that the requirements for environmental analysis, protection and mitigation for impacts to the environment were adequately addressed through conformance with adopted development regulations and applicable state and federal laws.

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### 2.2.4 RCW 58.17.035 - BINDING SITE PLANS

The Washington State Legislature authorized the binding site plan method of dividing property in RCW 58.17.035 as an alternative to the subdivision or short subdivision process. Binding site plans may be used to subdivide industrial or commercial land, land for the purpose of lease when no residential structures other than mobile homes or travel trailers are permitted, and land for condominium purposes. The City's Planning Director approves binding site plans administratively and then they are recorded with the County.

Figure 7: Foothills Trail



## 2.3 PREVIOUS CITY PLANNING WORK

### 2.3.1 SR 410 DRAFT MASTER PLAN – PREPARED BY THE UNIVERSITY OF WASHINGTON - 2005

In 2005, a University of Washington’s Master of Urban Planning studio assisted the City in creating a Draft SR 410 Master Plan. Building on recommendations in the 2005 Comprehensive Plan, the draft plan sought to address the challenges facing Buckley by spurring economic development within the Rainier Gateway Subarea and downtown business core. The draft planning process began with background research on the existing regulatory context for the Rainier Gateway Subarea to identify potential constraints and opportunities.

A steering committee was formed and meetings were held to focus concerns associated with the future use of the corridor. The committee helped to develop and prioritize design approaches for the corridor, which were prepared as conceptual site plans. Three designs were developed for the corridor based on the feedback from the steering committee including an open space alternative, a commercially developed alternative, and a mixed-use alternative. A community workshop was held to obtain feedback on the proposed designs, the results of which led to the preferred site plan for the corridor.

During the initial steering committee meeting as well as in the public workshops, Buckley residents showed strong support for accentuating the Foothills Trail with small commercial/residential buildings while keeping the area between the trail and SR 410 as open space. The preferred site plan included a realignment of the SR 410/SR 165 intersection and a mix of open space and commercial, with second floor office space and residential. Priorities for inclusion in the corridor based on the community workshop included:

- Locating new multi-family housing near existing multi-family housing;
- Installing removable bollards on Jefferson Avenue for holding a farmer’s market or community event in the Rainier Gateway Subarea;
- Relocation of the gazebo in future public buildings area (part of Civic Core); and
- An amphitheater north of Wally’s.

The preferred site plan for the Rainier Gateway Subarea was designed to act as a catalyst for economic development within Buckley’s downtown core. The redevelopment of the corridor into a mixed-use area is consistent with current retail trends and unites the Foothills Trail and Main Street, creating a destination area around the trail. Housing proposed will bring more residents into the downtown area, and pedestrian-oriented features will create a vibrant place for residents and tourists alike.

### 2.3.2 RAINIER GATEWAY SUBAREA DESIGN GUIDELINES – PREPARED BY THE UNIVERSITY OF WASHINGTON – 2013

In 2013, the City contracted the University of Washington’s Department of Urban Design & Planning to develop design guidelines and implementation plan for the Rainier Gateway Subarea. The team gathered data, researched best practices, and engaged the citizens of Buckley through public meetings in order to inform their understanding of Buckley’s economic conditions and community goals. After analyzing those findings, multiple recommendations were prepared for consideration by the City, the product being the Rainier Gateway Subarea Design Guidelines and Implementation Plan.

The design guidelines were a tool for shaping commercial and civic development within the Rainier Gateway Subarea. The guidelines seek to maintain the character of Buckley, while maximizing the economic and social potential of the Rainier Gateway Subarea. The guidelines required that new development along the Rainier Gateway Subarea reflect the historic character of Main

Street, while also including provisions for providing access points along the trail to new businesses, housing and recreational opportunities.

The design guidelines and implementation plan separated development into three phases, which are adapted and built upon in the Rainier Gateway Subarea Plan. The first phase focused on creating a gateway between the Rainier Gateway Subarea and Main Street that promotes engaging, compatible, and complementary economic development in both areas. Phase I of the implementation plan included provisions for expanding the visual recognition and prominence of the Foothills Trail, for example using a series of consistent urban design elements to create continuity along the trail and Main Street. On-street parking was proposed along River Road with a clustering of development to maximize buildable land while maintaining opportunities for open space.

## 2.4 Foothills Trail

The vision of the Foothills Trail began with Dr. Douglas “Doc” Tait in Buckley, over 30 years ago. Doc Tait envisioned a Rails-To-Trails project that would connect Buckley to Tacoma, passing through South Prairie, Wilkeson, Carbonado, and Orting along the way. In 1983, the Foothills Rails-to-Trails Coalition was formed to develop the Foothills Trail along the route of the abandoned Northern Pacific Railroad. After years of controversy and lawsuits over the ownership of railroad parcels, the first mile of trail was constructed in Orting. After seeing the success of this trail segment, Pierce County began work connecting the trail to Puyallup. The City of Buckley, however, was not included in early trail plans and began work on its own Foothills Trail, a 2-mile segment of paved trail meandering through the heart of the City. Plans currently exist for the County to connect Buckley’s trail into the 15 miles of completed trail spanning from Puyallup to South Prairie. Eventually, the Foothills Trail will connect Buckley and Mount Rainier National Park to the waterfront of Tacoma’s Commencement Bay, just as the Northern Pacific Railroad did. When complete, the trail will cover more than 28 miles.

In the future, the Foothills Trail will connect Buckley to Enumclaw and King County via a bridge over the White River. The Washington State Department of Transportation (WSDOT) is currently replacing the steel-truss Meridian Street Bridge in Puyallup and has identified the potential to preserve the old bridge for use on the Foothills Trail between Enumclaw and Buckley across the White River.

The Foothills Trail has a special significance in Buckley, with its roots firmly planted in the City’s history. The two-mile paved stretch of trail is the most heavily used park in the City. The plans for connecting Buckley’s trail to South Prairie and Enumclaw will only further enhance its use, and make it a regional destination.

## 3 RAINIER GATEWAY SUBAREA PLAN

### 3.1 EXISTING USES

Existing buildings and uses are present within the Rainier Gateway Subarea, and as such, the development plan for the corridor must consider them in the designation of future uses. The following uses currently exist within the Rainier Gateway Subarea, as shown in Figure 7.

1. Foothills Trail – The Foothills Trail, located within the Rainier Gateway Subarea, is a community-gathering place for the City as well as a recreational destination. Locally, the trail is dubbed the new “Main Street,” and is the most used park in the City. The Trail has the potential to be a regional recreational destination with the completion of the extensions to South Prairie and Enumclaw.
2. Park & Ride – This unpaved lot serves as an unofficial park-and-ride for the City of Buckley.
3. Veterans Memorial – The Buckley Veterans Memorial monument was built in 2010 to be a place of remembrance for those who served in the armed forces.
4. Buckley Youth Center – The Buckley Youth Activity Center is a safe place for kids to stay after school, with recreational activities such as pool, air hockey, video games, foosball, and other organized activities. The Youth Center also provides free tutoring for students in grades K-12.
5. Skateboard Park – Built in 2014, the Buckley Multi-Use Skateboard Park features a playground area with equipment, a viewing area and a skate park, providing facilities for skateboarders, roller bladers, and BMX bike riders.
6. U.S. Post Office – The Buckley U.S. Post Office is located within the Rainier Gateway Subarea and serves the City of Buckley.
7. Public Art – Several public art installations are located along the Foothills Trail in the Rainier Gateway Subarea and are a highly valued community asset. A local artist donated the chainsaw carvings in 2006, which are carved from three 40-year old Coulter pines that were on the verge of falling down.
8. Buckley Wine & Spirits and Barber Shop – A wine and liquor store and barber shop are located within a small retail development in the Rainier Gateway Subarea.
9. Welcome Sign – The welcome sign to Buckley directs visitors to the historic district and business district.
10. Buckley Library – The Buckley Library, located within the Rainier Gateway Subarea, serves northeastern Pierce County, providing many benefits to the community.
11. Memorial Trees – Several significant trees and trees planted in memoriam are located and signed within the Rainier Gateway Subarea, immediately north and south of Main Street, and should be preserved from future development.
12. Del’s Feed and Farm Supply – A farm and ranch supply store currently exists within the Rainier Gateway Subarea. The store includes an outdoor space for displaying lumber, fencing, and other farm supplies.
13. Buckley Hall – Buckley Hall is an old feed store that was donated to the City and renovated into a multi-use facility. The City uses the building for community activities such as yoga, aerobics, and dance classes, as well as youth activities and programs. The facility is also rented for private events such as weddings or parties.



14. Foothills Historical Society – Founded in 1981, the Foothills Historical Society is responsible for preserving the local history of the towns in the foothills of Mount Rainier. Located across the street from the museum, the presence of the Foothills Historical Society in the Rainier Gateway Subarea provides historical and cultural benefits for tourists and residents alike.
15. Foothills Museum – The Foothills Historical Museum was built in 1927 and was originally a funeral home. The Foothills Historical Society, in 1985, converted it to a museum, located along the Foothills Trail in the Rainier Gateway Subarea.
16. River Avenue Basketball Courts and Playground – This Park consists of a basketball court, an outdoor playground apparatus, and a restroom.
17. Wally's Drive-In – Wally's White River Drive-In, a local staple in the plateau, is an old-fashioned drive-in restaurant.
18. White River Family Care – White River Family Care is a family practice doctor's office serving the City of Buckley.
19. Log Show Grounds – The Buckley Log Show takes place each year at the Log Show Grounds located within the Rainier Gateway Subarea. Various competitions are held, with entrants participating in events such as the ax throw, hot saw, or tree topping.
20. Army National Guard Armory – The Washington Army National Guard armory is located within the northern portion of Rainier Gateway Subarea.
21. Buckley Cemetery – The Buckley Cemetery is located at the northeastern border of the Rainier Gateway Subarea.

## 3.2 DEVELOPMENT PLAN

When the Draft Rainier Gateway Subarea Plan was developed in 2005, community outreach events were held to create a preferred development approach for the corridor. Some of the considerations included in the preferred development approach included:

- Preserving open space while allowing for some commercial development;
- Providing a park and ride facility near the trail;
- Adding more housing in the downtown area and along the trail; and
- Preserving trees and vegetation in the corridor, especially the memorial trees north and south of Main Street.

Another factor important to the community, as identified in the Comprehensive Plan, is the desire to position the community as a “gateway to Mount Rainier” and a destination for tourists, and residents alike, interested in outdoor recreational activities.

The development plan for the Rainier Gateway Subarea (see Figure 8), takes into consideration the communities preferences for the corridor, and the development and uses currently located within the Rainier Gateway Subarea, and depicts where future development may take place. The sections below break down the implementation steps necessary to facilitate the vision for the Rainier Gateway Subarea.

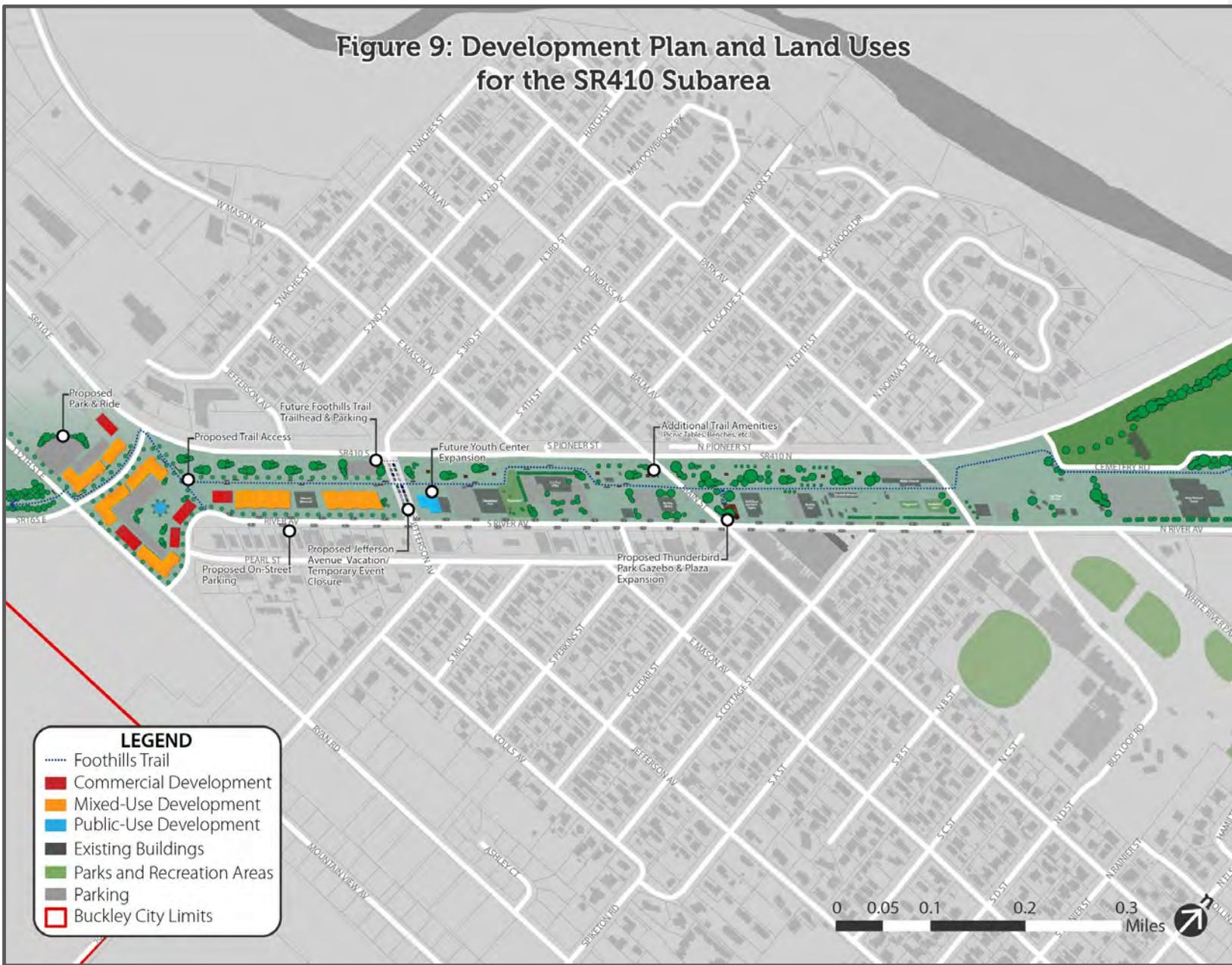
### 3.2.1 MAIN STREET GATEWAY

In the 2013 Rainier Gateway Subarea Design Guidelines and Implementation Plan for the Rainier Gateway Subarea, subdivisions were proposed for parcels in what was dubbed the “Main Street Gateway.” While the subdivision of property in Main Street Gateway would facilitate economic development and enhance the connection between the Rainier Gateway Subarea and the historic downtown, existing development in Main Street Gateway, as well as significant cultural amenities make it an impractical location for future development. The U.S. Post Office, the two retail spaces, the Pierce County Library, several public art installations and memorial trees are all valuable uses to the City located in the Main Street Gateway and are to be retained in the Rainier Gateway Subarea.

This Subarea Plan will propose no subdivisions for the Main Street Gateway, and will instead focus on creating open spaces and recreational opportunities, as residents and tourists have identified they would like to see in the area. This phase is considered the “amenity phase” as the emphasis is on creating recreational and cultural amenities that will draw in residents and tourists. Figure 9 depicts how the Main Street Gateway could continue to be enhanced with recreational and cultural amenities.

Within the Main Street Gateway, the focus will be providing additional trail amenities as funds are available. Future expansions to the Youth Center will provide additional recreational opportunities within the Rainier Gateway Subarea, as well as essential community services. The City plans to construct a memorial to Doc Tait in the area southwest of the gazebo at Thunderbird Park. The City could also consider expanding Thunderbird Park in order to create an additional gathering place within the Rainier Gateway Subarea that can be rented for private events or as a rest area for trail users. Connecting the plaza and memorial to the trail is essential in enhancing the connection between the trail and the historic Main Street area.

**Figure 9: Development Plan and Land Uses for the SR410 Subarea**



- LEGEND**
- ..... Foothills Trail
  - Commercial Development
  - Mixed-Use Development
  - Public-Use Development
  - Existing Buildings
  - Parks and Recreation Areas
  - Parking
  - Buckley City Limits

0 0.05 0.1 0.2 0.3 Miles

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### 3.2.2 SOUTH GATEWAY

With the realignments of Ryan Road/112<sup>th</sup> Street, SR 165, and SR 410, the South Gateway will now serve as the first entrance to Buckley for visitors coming from the west. With much of the South Gateway undeveloped, it is the ideal location for the mixed-use commercial and residential development proposed in both the 2005 Draft Subarea Plan and the 2013 Rainier Gateway Subarea Design Guidelines and Implementation Plan. Figure 10 depicts how the South Gateway could be developed with mixed-use and commercial development, leaving the portion of the Rainier Gateway Subarea west of the trail as open space and preserving existing uses, such as the Veterans Memorial.

As proposed in 2013, the City should amend the binding site plan for Parcel 800005-0210. The City could create a maximum of eight lots within this parcel, not including the Veterans Memorial, with a minimum frontage of 40 feet and minimum lot size of 3,000 square feet. The City may choose to either amend the binding site plan for this parcel in an effort to market the parcel to potential developers, or wait until there is interest.

The City should also create a binding site plan for Parcel 800005-0200. The City should adjust the lot line to match that of Parcel 800005-0210, leaving the portion west of the Foothills Trail and Parcel 800005-0220 as open space, and dividing the remainder into a maximum of nine lots.

After amending the binding site plan for Parcel 800005-0210 and creating a binding site plan for Parcel 800005-0200, the new parcels will be marketed according to the Marketing Plan in Section 3.5.

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### 3.2.3 NORTH GATEWAY

The final development phase encompasses all parcels not already included in Phase I or Phase II. Significant development is not proposed for the North Gateway, and as such, the City may initiate the following projects whenever practical. The focus of this phase is to create open spaces and links between existing development, new development and the Foothills Trail. The City may choose to market Parcel 8000050010 for private development, or use it as open space or other public facility.

## 3.3 DESIGN GUIDELINES

The Rainier Gateway Subarea Design Guidelines are intended to guide land use and site development within the Rainier Gateway Subarea. The Design Guidelines build on the work completed by City and the University of Washington in 2013, and are included in Appendix A of this plan. The Design Guidelines emphasize architectural context that is in keeping with the character of Buckley's historic Main Street, and respecting the existing pedestrian scale of the district. Provisions are included providing entrances on both street frontages at the Foothills Trail. The Design Guidelines specify architectural elements that reflect the history of Buckley and enhance the rural and natural features that surround the City.

Figure 10: Development Plan for the South Gateway

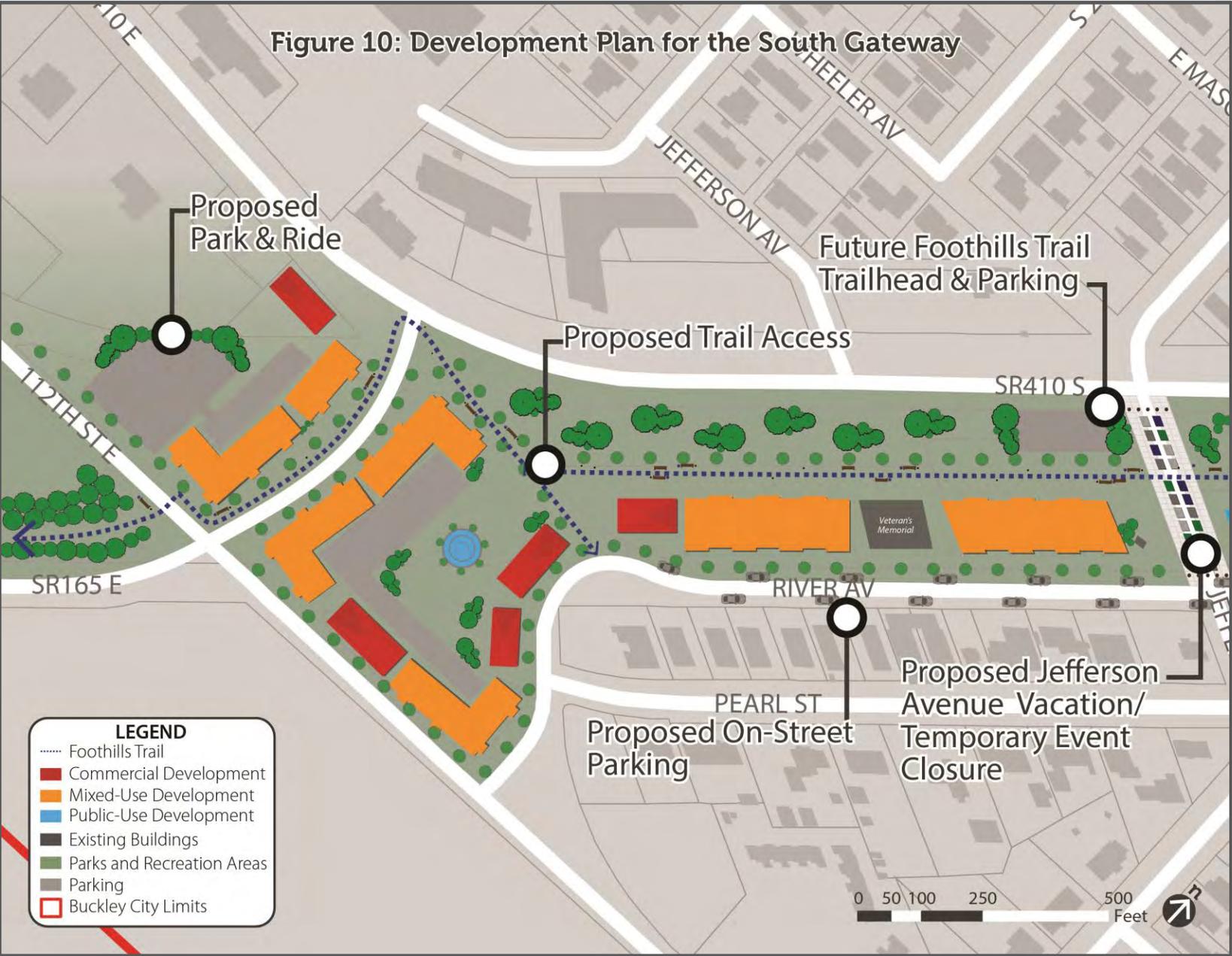


Figure 11: Development Plan for the Main Gateway

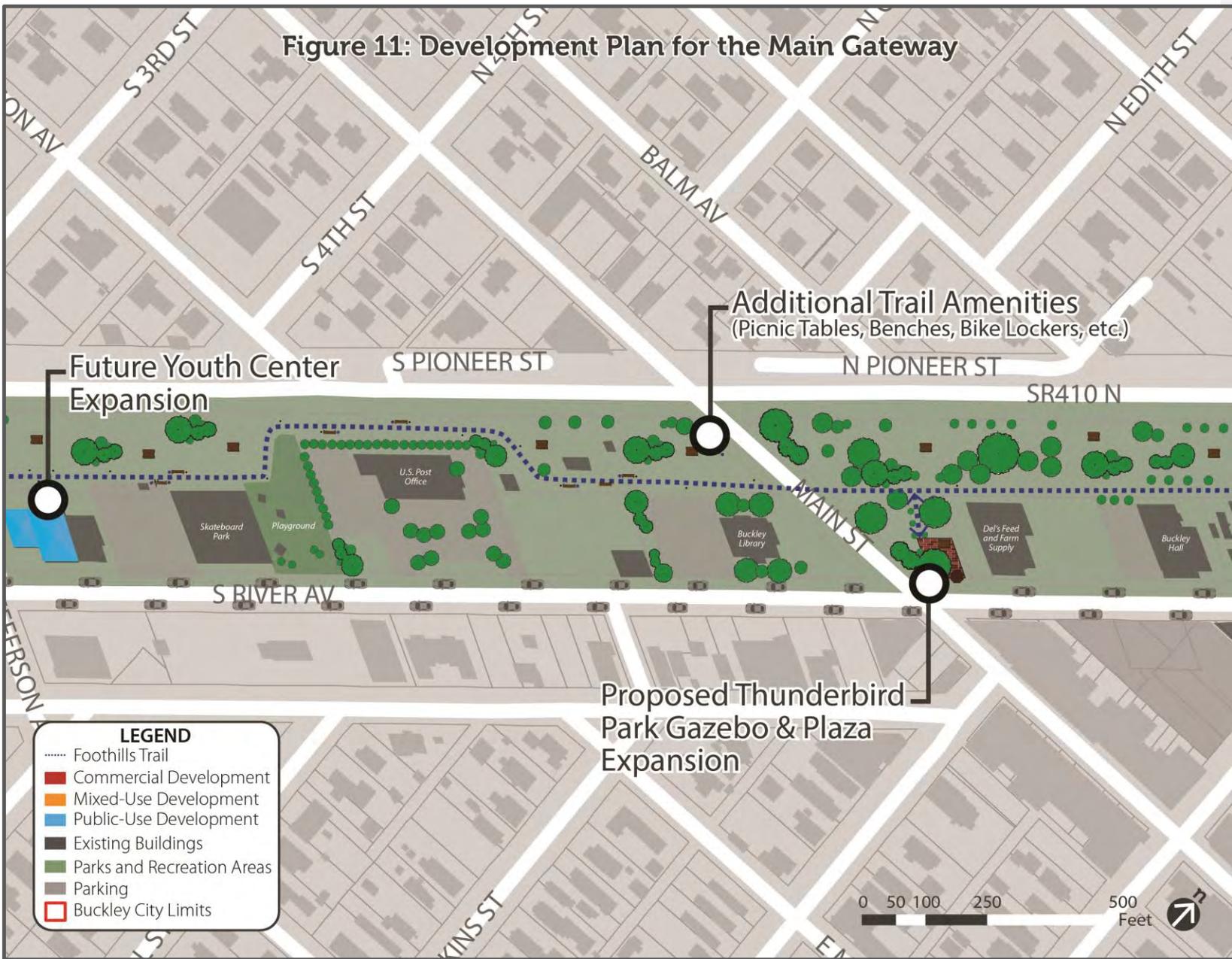
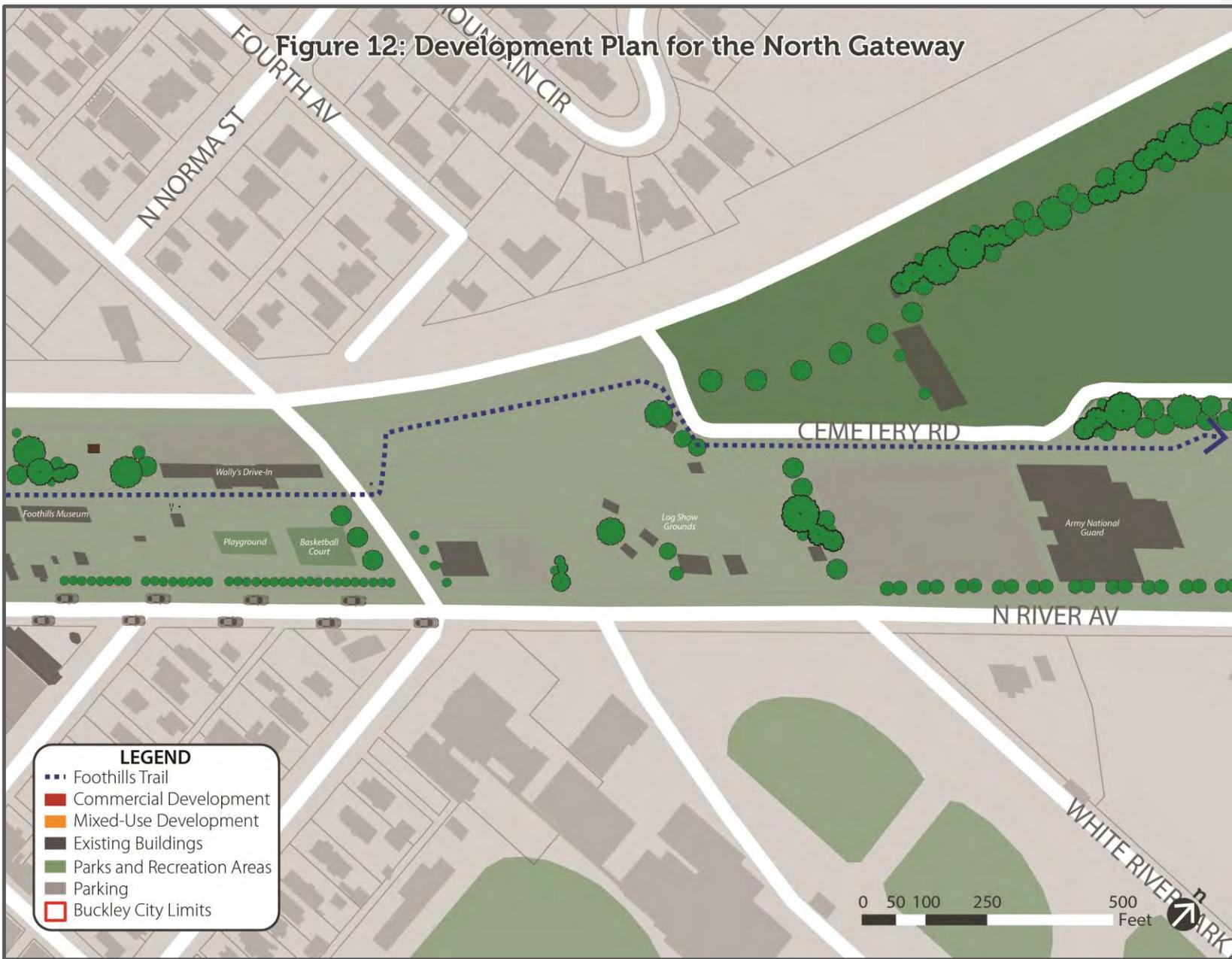


Figure 12: Development Plan for the North Gateway



- LEGEND**
- Foothills Trail
  - Commercial Development
  - Mixed-Use Development
  - Existing Buildings
  - Parks and Recreation Areas
  - Parking
  - Buckley City Limits

0 50 100 250 500 Feet

## 3.4 IMPLEMENTATION

### 3.4.1 OPEN SPACE IMPROVEMENTS

The City incorporated many open space improvements along the Foothills Trail following the 2013 Rainier Gateway Subarea Design Guidelines and Implementation Plan. Uniform lighting along the trail allows extended use beyond daytime hours. Benches and landscaping along the trail helps to create appealing spaces for people to meet and spend time in the Rainier Gateway Subarea. The City indicates in its 2015 Comprehensive Plan desire to construct a splash park and climbing wall within the Rainier Gateway Subarea. The City may incorporate additional open space improvements that continue to build a visual recognition of the Foothills Trail and enhance interest in the area.

#### 3.4.1.1 PLAZAS

Plazas and open spaces should be provided to link trails with commercial areas. Plazas provide places for passive recreation, and provide cultural benefits by creating a space for people to gather and hold community events. Existing small plazas should be enhanced to link the trail to downtown.

**Figure 13: Thunderbird Park Gazebo and Plaza**



**Existing plazas, such as the one at Thunderbird Park, could be enhanced to connect the trail to downtown. Decorative paving and landscaping extending from River Avenue and the Foothills Trail could provide a visual connection and provide a place of respite for trail users.**

### 3.4.1.2 ART INSTALLATIONS

Additional art installations should be encouraged along the Foothills Trail. Public art provides visual interest for trail users. The art installations should reflect the history of Buckley, and be compatible with existing art installations along the trail. Art installations located in the Rainier Gateway Subarea could act as a way to set Buckley apart from other Foothills Trail communities, and highlight the character of the City.

**Figure 14: Chainsaw Art Installations**



**Chainsaw art installations, donated by a local artist in 2006, reflect Buckley’s logging history.**

### 3.4.1.3 SIGNS

Currently, signs for the Foothills Trail are smaller and not of the same style as signs throughout the City for the Main Street shopping area. Consistent sign design throughout the City would help foster a visual identity, and emphasize the trail as a major destination in Buckley. Larger signs, of similar style to existing signs, would be easier for visitors to see and recognize, increasing their likelihood of using the trail.

## 3.4.2 STREET IMPROVEMENTS

The City should consider street improvements to improve access to the Rainier Gateway Subarea. Improvements to River Road, such as on-street parking, bicycle racks, and wayfinding can help to direct people to the Rainier Gateway Subarea and enhance the connection between the Rainier Gateway Subarea and Main Street.

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#### 3.4.2.1 ON-STREET PARKING

On-street parking along River Road should be installed consistent with the Rainier Gateway Subarea Design Guidelines and City of Buckley Development Guidelines and Public Works Standards. On-street parking provides economic development benefits to the Rainier Gateway Subarea by increasing vehicular access to the trail, as well as shops and restaurants, and creates a buffer between pedestrians and traffic.

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#### 3.4.2.2 BOLLARDS

Removable bollards at the intersections of Jefferson Street and SR 410 and Jefferson Street and River Road would allow the road to be closed periodically for events. The City could make use of the closure on a weekly basis during the summer for a farmers market, as well as holding other local events throughout the year. The use of Jefferson Street for events and farmers markets would help bring residents and tourists to the Rainier Gateway Subarea, and add to its importance as a place of gathering in the community.

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#### 3.4.2.3 WAYFINDING

Wayfinding is essential for making Buckley a destination, rather than a place to travel through. Providing wayfinding on the trail that directs pedestrian and bicycle traffic to Main Street and other areas of interest would provide economic benefits to downtown businesses, and would help to increase the City's regional visibility.

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#### 3.4.2.4 BIKE RACKS

Installing bike racks along the trail and on Main Street would encourage trail users to stop and enjoy Buckley shops, restaurants, and events, and increase the likelihood of people returning to Buckley.

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#### 3.4.2.5 TRAILHEAD

With the completion of the South Prairie to Buckley section of the Foothills Trail, a signed trailhead for the trail can help to increase Buckley's visibility in the region, and the use of the trail, locally. A trailhead in Buckley would provide a place for users coming from King County to park and access the trail.

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### 3.4.3 STOREFRONT IMPROVEMENTS

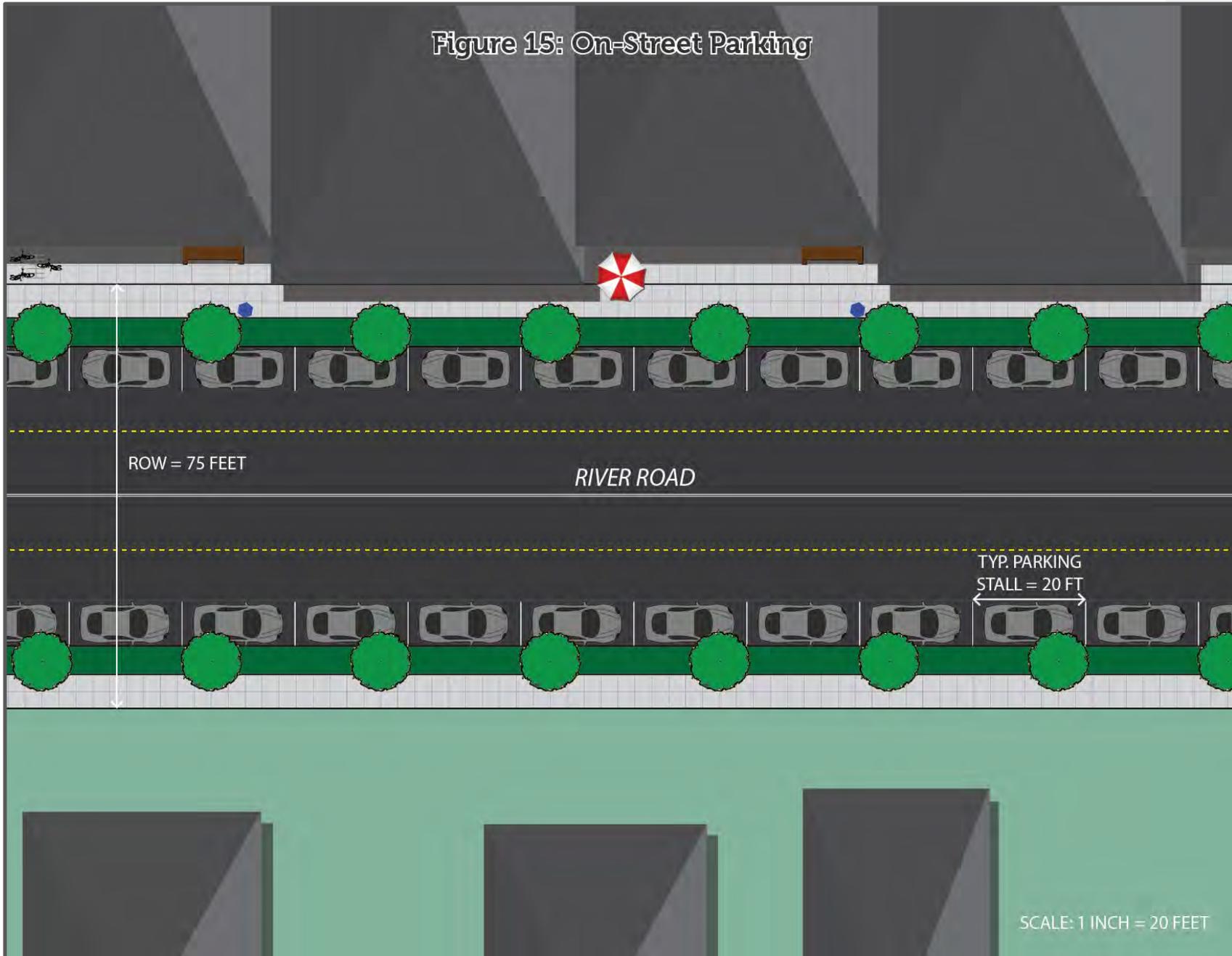
The City should consider working with local Main Street businesses to enhance their storefronts and improve the overall aesthetics of Main Street. Minimizing the amount of vacant storefronts on Main Street, and their appearance of vacancy, and keeping business lights on during specified hours would help to make Main Street a more lively area, and spur additional development within the Rainier Gateway Subarea.

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#### 3.4.3.1 LIGHTING

The City should work with businesses on Main Street to establish uniform times during the day for stores to keep their storefront lighting on. Dark stores give off the appearance that everything is closed, and send a message to people whom passersby that this is not a vibrant shopping area. Keeping lighting on will encourage people to stop in Buckley, and check out shops and restaurants that are open.

Figure 15: On-Street Parking



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### 3.4.3.2 VACANT STOREFRONTS

Vacant storefronts create blight and hurt the economic viability of other Main Street businesses. Vacant storefronts should also have their appearance of vacancy minimized. Artists could display their work in vacant storefronts, bringing themselves visibility and improving the appearance of the area.

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## 3.4.4 ECONOMIC DEVELOPMENT INCENTIVES

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### 3.4.4.1 PROPERTY TAX SUBSIDY/ABATEMENT

The City may offer property tax discounts or complete property tax relief for a specified time to businesses or developers interested in locating within the Rainier Gateway Subarea.

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### 3.4.4.2 ADMINISTRATIVE FEE REDUCTIONS

Administrative fees such as permit fees, impact fees, and water/sewer connection fees all increase the upfront construction cost of development. Reducing fees would save developers money and encourage development within the Rainier Gateway Subarea. Administrative fee reductions could be provided as a reimbursement for developers once they begin construction or start installing infrastructure. A percent of the fees could also be waived at the time of application.

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## 3.4.5 SEPA CATEGORICAL EXEMPTION THRESHOLDS

The City of Buckley will be raising the categorical exemption limits under SEPA for new construction to the maximum specified under WAC 197-11-800(1)(d). This will allow for a larger range of development to occur within the Rainier Gateway Subarea without the delay and additional cost of threshold determination and EIS requirements under SEPA.

Currently the categorical exemption limits in Buckley for multi-family residential is four units, and the maximum exemption for commercial development is 8,000 square feet. The maximum exemption under SEPA for multi-family residential is 30 units, and the maximum exemption for commercial development is 30,000 square feet.

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## 3.5 MARKETING PLAN

The goal of the marketing plan is to disseminate information on the Rainier Gateway Subarea properties to potential buyer/developers through a variety of methods in an effort to generate interest and development leads. Techniques to generate interest include development visuals, together with property and community data and demographics.

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### 3.5.1 MARKETING COLLATERAL

Prototype Development concepts will be prepared for the South Gateway area illustrating development potential to convey the vision of the Rainier Gateway Subarea to interested buyer/developers. The City should evaluate the existing parcel configurations and determine if modifications to the boundaries, through either boundary line

adjustments or a revised Binding Site Plan, are appropriate in advance of property sale. The concepts will build on those prepared to date and convey the opportunities and attributes of the area.

Using the development concepts, marketing flyers should be prepared describing the properties for sale and their development potential both in graphic and narrative form. The flyers could be posted on bulletin boards at City Hall, the Library, and Community Center among other key frequently visited areas.

The City should develop a page on its website extolling the Rainier Gateway Subarea plan vision and overall economic development incentives available to buyer/developers. The marketing flyers and development concepts will be useful visuals for the web format.

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### 3.5.2 MARKETING ACTIVITIES, EVENTS AND VENUES

Community gatherings and special events are a good opportunity for the City to promote the Rainier Gateway Subarea Plan and Citywide efforts for economic development. The use of kiosks and promotional sponsorships at City events such as the log show, farmers market, groundbreaking ceremonies, etc. can attract attention to the Rainier Gateway Subarea and demonstrate City efforts for improvements. This can include attractive display of promotional materials and/or staffing by informed personnel. The City should take every opportunity to commemorate and promote improvements in the Rainier Gateway Subarea with a groundbreaking ceremony.

The City should consider contracting with the commercial real estate brokerage community to assist in marketing the Rainier Gateway Subarea. Attending regional retail conferences and events or contracting with the real estate brokerage community to market the properties at such events would also assist in promoting the properties.

The City should also establish a Rainier Gateway Subarea implementation stakeholder group or staff that is charged with ensuring that the implementation activities are occurring and that the information is disseminated in a timely manner when requested from a buyer/developer.

## 3.6 FUNDING

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### 3.6.1 CAPITOL PROJECT FUNDS/ENTERPRISE FUNDS

The City may use revenues deposited into capital improvement funds, enterprise funds, and other funds designated for uses in line with the plan for the Rainier Gateway Subarea. Enterprise funds encompass any activity for which a fee is charged to external users for goods and services. A common source of revenue for enterprise funds are City-owned utility services, such as water, sewer, garbage, or stormwater. Capital improvement funds and tourism funds may be used for improvements in the Rainier Gateway Subarea, and are generated through revenues received by the City from real estate and lodging excise taxes.

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### 3.6.2 LOCAL REVITALIZATION FINANCING

The Local Revitalization Financing Program allows cities to create “revitalization areas” and allocate increases in local sales and use taxes, and local property taxes, to fund bonds issued for local public improvements within the revitalization area. While the State is no longer accepting applications for state contribution, the City may use the program to increase tax revenues specifically for the Rainier Gateway Subarea.

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### 3.6.3 MAIN STREET TAX CREDIT INCENTIVE PROGRAM

The [Main Street Tax Credit Incentive Program](#) is a way for businesses to redirect their business and occupation taxes to their own community. The downtown organization receiving the fund must be a part of the State's Main Street Program. Businesses can then direct that their tax payment go to the downtown organization, and receive a credit worth 75 percent of their contribution.

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### 3.6.4 PUBLIC/PRIVATE PARTNERSHIPS

Public/Private Partnerships (PPPs) are alliances formed between a government agency and private developers to achieve a common purpose. PPPs typically begin with a conceptualization process, and then private partners are selected through a competitive bid process. The entities involved then define project elements, and develop an implementation process. The process results in benefits for all parties involved; public sector entities can leverage and maximize public assets and private sector entities can be given greater access to land and infill sites, and receive more support through the development process. The City may look into creating a Public Development Authority to assist in facilitating the partnership.

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### 3.6.5 PUBLIC DEVELOPMENT AUTHORITY

One method of facilitating public/private partnerships is for the City to create a Public Development Authority (PDA). A PDA is authorized under RCW 35.21.170 which allows cities to establish "public corporations, commissions, or authorities." The creation of a PDA is useful for engaging in activities that are outside of the normal scope of City operations. The PDA would be legally separate from the City, and would be able to develop properties together with private property owners and developers. A PDA can derive its revenue from the City through lease and operation payments and user fees, or through grants or donations.

## 4 POLICIES AND RECOMMENDATIONS

POLICY/RECOMMENDATION	SUPPORTING POLICIES/CODES	IMPLEMENTATION METHOD
4.1 The Rainier Gateway Subarea is a place where people meet, recreate, and is an important open space area that gives the City a pleasant atmosphere. Commercial and mixed-use development is planned to be erected between the Foothills Trail and River Avenue. The area between SR 410 and the trail would remain as open space.	Comprehensive Plan Policy 3.4.3 Rainier Gateway Subarea Draft Master Plan Rainier Gateway Subarea Design Guidelines	Utilize strategies from Section 3.4, Implementation Plan, and funding sources from Section 3.6, to facilitate the City's vision for the Rainier Gateway Subarea. Adopt Design Guidelines, included in Appendix A, for the Rainier Gateway Subarea and require development to incorporate design principles identified through amendments to BMC §19.51 Highway 410 Design Review District, as specified in Section 5.1. Remove barriers to development by simplifying the binding site plan, as specified in Section 5.2, and raising the categorical exemption thresholds for SEPA review, as specified in Section 5.3. Market City-owned parcels for sale consistent with Section 3.5, Marketing Plan.
4.2 Encourage retail uses that cater to outdoor recreationalists.	Comprehensive Plan Policy 4.2.4 BMC §12.04 SEPA	Provide Economic Development Incentives to developers who agree to develop retail uses that focus on outdoor recreation, as specified in Section 3.4.4 of the Implementation Plan. Raise the Categorical Exemption limit for SEPA to allow for larger building footprints without the additional time and expenses for SEPA review. Market City-owned parcels for sale consistent with Section 3.5, Marketing Plan.
4.3 Require all development within the Rainier Gateway Subarea to incorporate neighborhood character and design principles consistent with Rainier Gateway Subarea Design Guidelines into building and site design.	Comprehensive Plan Policy 2.1.1 Comprehensive Plan Policy 3.2.3	Modify BMC §19.51 Highway 410 Design Review District to require development to comply with the Rainier Gateway Subarea Design Guidelines for property within the Rainier Gateway Subarea.
4.4 Concentrate new retail development in the Rainier Gateway Subarea.	Comprehensive Plan Policy 1.1.1 BMC §12.04 SEPA	Provide Economic Development Incentives to developers who agree to develop retail uses in the Rainier Gateway Subarea, as specified in Section 3.4.4 of the Implementation Plan. Raise the Categorical Exemption limit for SEPA to allow for larger building footprints without the additional time and expenses for SEPA review. Market City-owned parcels for sale consistent with Section 3.5, Marketing Plan.
4.5 Install removable bollards on Jefferson Avenue between SR 410 and River Avenue to allow for street closures and the use of the	Comprehensive Plan Policy 4.1.1 Rainier Gateway Subarea Draft Master Plan	Utilize funding strategies from Section 3.6 to install removable bollards, as specified in Section 3.4.2.2 of the Implementation Plan.

	street for community events.		
4.6	Develop a visual link between the trail and downtown by using elements such as pavement materials or landscaping. For example, the existing small plaza could be enhanced to help connect the trail with downtown.	Comprehensive Plan Policy 4.2.2	Utilize funding strategies from Section 3.6 to install open space improvements, as specified in Section 3.4.1 of the Implementation Plan.
4.7	Encourage mixed-use developments in commercial zones.	Comprehensive Plan Policy 1.1.1	Utilize binding site plans to allow for easier subdivision of parcels in the Rainier Gateway Subarea for mixed-use and commercial development. Revise the binding site plan approval procedure to allow binding site plans to be approved administratively as a Type A-1 decision. Market City-owned parcels for sale consistent with Section 3.5, Marketing Plan.
4.8	The Rainier Gateway Subarea should be designated with the county as a center of local importance and called the Rainier Gateway Center.	Comprehensive Plan Policy 3.4.3 Comprehensive Plan Policy 1.1.2 Pierce County Countywide Planning Policies (CPPs)	Adopt the Rainier Gateway Subarea as a CoLI by documenting how the area meets the design features required by the Pierce County Comprehensive Plan Policies, and including plans, such as the Rainier Gateway Subarea Plan, and other information that supports the designation in the City's Comprehensive Plan. The Rainier Gateway Subarea should be delineated on a map as a CoLI in the Comprehensive Plan. The City should then provide notice to the Pierce County Regional Council of its intent to adopt a CoLI.
4.9	Minimize the number and appearance of vacant storefronts.	Comprehensive Plan Policy 3.1.2	Incorporate incentives for minimizing the appearance of vacancy as specified in Section 3.4.3.2 of the Implementation Plan.
4.10	Plazas and open space should be provided to link trails with commercial areas, and to provide places of respite for trail users to stop and visit downtown.	Comprehensive Plan Policy 1.6.4 Comprehensive Plan Policy 4.2.3	Utilize funding strategies from Section 3.6 to create plazas and open spaces, and enhance existing open spaces, as specified in Section 3.4.1.1.

## 5 REGULATIONS

The following regulations will help to fulfill the goals and policies outlined in Section 4 of the Rainier Gateway Subarea Plan.

### 5.1 BMC §19.51 HIGHWAY 410 DESIGN REVIEW DISTRICT

Ordinance **(ordinance number)** is adopted on **(date)** and amends BMC §19.51 Highway 410 Design Review District to require all properties within the Rainier Gateway Subarea to comply with the new Rainier Gateway Subarea Design Guidelines. The ordinance adopting the proposed amendments to the regulations is included in Appendix B of the Rainier Gateway Subarea Plan.

### 5.2 BMC §18.36 BINDING SITE PLAN

Ordinance **(ordinance number)** is adopted on **(date)** and amends BMC §18.36 Binding Site Plan to require Type A-1 administrative approval. Previously, a binding site plan was required to go to hearing examiner for public hearing and review, before final approval. The amendment to BMC §18.36 will make it easier for applicants to make use of the binding site plan approach to divide commercial land for sale or lease.

### 5.3 BMC §12.04 SEPA

Ordinance **(ordinance number)** is adopted on **(date)** and amends BMC §12.04 SEPA to reflect the 2014 rulemaking and new maximum categorical exemption thresholds for minor new construction, allowed under WAC 197-11-800(1)(d). The revisions to the City's SEPA ordinance will help to remove barriers to development by allowing for a larger range of development to occur within the City without the delay and additional cost of threshold determination and EIS requirements under SEPA. The City has determined that the increased thresholds adopted are appropriate for Buckley's localized conditions, in light of increased environmental protections in place such as RCW 36.70A, RCW 90.58, and other laws and procedures for environmental protection.

# CITY OF BUCKLEY

P.O. Box 1960 ♦ Buckley, WA 98321 ♦ (360) 829-1921 ext. 7801

[www.cityofbuckley.com](http://www.cityofbuckley.com)



## *Design Guidelines*

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### Acknowledgements

Based on the work prepared by Buffalo Design Inc. and done by the City of Buckley, Washington, October, 1991, by the City Council and Architectural Review Board consisting of members Mayor Kathleen Sandor, Meagan Rhoades, Gary Brevik, Gene Smith, Les Holly, Marty Sandor, Jerri Dever, Pat Johnson, Gary Raihl, Laverne Harris, Ann Gibson, Zoe Krieger, Martha Olsen, and Irene Vanderhoof, with assistance from the Foothills Historical Society

The work was updated in 2008, and is updated again in 2016, following work by AHBL, and review and updates by the planning commission and city council.

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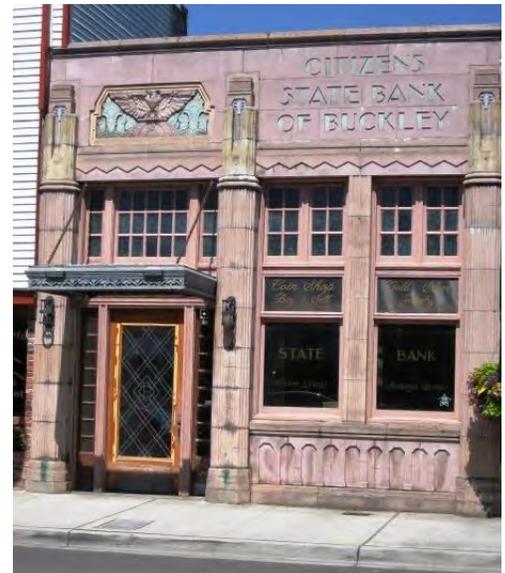
**I. Welcome**  
**I.A Introduction**

The city of Buckley, originally named Perkin’s Prairie in 1882, is one of the oldest Pierce County communities, and it has long been a crossroads of activity for railroad, lumber, agriculture and mining interests. Traces of each industry are still present in the buildings and streets of the city.

The plateau on which Buckley is sited was an early trading route through the Washington Territory, with the Naches Pass Trail and Military Road passing through the area. Later, in the 1880’s, the Northern Pacific Railroad established a siding at White River as a part of the transcontinental rail link to the east coast.

Recognizing that the railroad had the potential of securing more business interests, the town was renamed Buckley in 1888 for a division superintendent of the railway. Buckley was incorporated by the electorate in 1889.

Although the anticipated railroad boom never fully materialized, the town was physically shaped by the railway. Especially at River Avenue, Ryan Road, and SR 165, irregular angles on the dominant street grid have created confusing intersections. Simplifying this intersection has been an on-going project.



Through the late nineteenth and early twentieth centuries Buckley grew slowly, remaining somewhat isolated from communities in the valley below. The town served as a local center of trade for farmers and loggers.

The business district, gutted by fire in 1892, and again in 1923, began to replace its simple wood framed storefronts with brick and cast iron buildings, taking on the appearance of an established, permanent city. Many of these buildings are good examples of the commercial architecture of the day.

As the influence of the automobile grew after World War II, and the markets and services in Tacoma and other towns became more accessible, Buckley’s downtown came under increasing economic pressure. Suburban supermarkets built in larger, adjacent towns in the fifties and sixties brought difficult times to downtown business, the traditional hub of community life.

In the seventies and much of the eighties, Buckley’s relatively remote location surrounded by farms, helped keep it insulated from the suburban sprawl that blurred the identities of other small towns in Western Washington. However, as the population of Pierce County increases and farms are sold for other uses, this insulation is beginning to disappear. Change is inevitable.

In 1982 the Burlington Northern Railway ended service on the tracks, and the railroad bed was acquired by the City and Pierce County forming the Foothills Trail. The 25-mile-trail has become a

popular recreational destination for bicyclists, horse enthusiasts, and hikers. The trail currently consists of 15 miles of paved non-motorized trail from South Puyallup to South Prairie, with a two mile paved section in Buckley. Plans exist to connect the Foothills Trail in Buckley to the main paved trail ending in South Prairie, as well as to continue north through the City to Enumclaw in King County.

The city of Buckley welcomes the opportunities this growth should make possible, but realizes it is important to maintain the character and quality of life, as well as maintaining the environmental biodiversity that make the city a desirable place to live.

Buckley has established a Design Review Committee charged with the review of proposals for restoration and new development in key districts, and in other areas as directed by the City Council. Proposed projects submitted in the Design Review Committee must meet the development standards outlined in the guidelines.

The 2005 Comprehensive Plan identified recreational aspects of the community as being a key to promoting economic development.

The SR 410 railroad right-of-way corridor, bounded by River Road to the southeast, SR167/Ryan Road to the southwest, and Park Avenue to the northeast, is referred to as the Rainier Gateway Subarea Plan has received close attention in the last few years. In 2013 students from the University of Washington, after careful consultation with the community through surveys and meetings, presented ideas for the Rainier Gateway, as well as proposing development of parks' land. These ideas were an extension of the 2005 Comprehensive Plan.



Buckley's 2015 Comprehensive Plan, building on the 2005 plan, offers guidelines to bring the city of Buckley through the 21st Century, developing the city without losing its small town character.

## **I.B Purpose.**

While the city of Buckley welcomes the opportunities this growth may make possible, it is important to maintain the character and quality of life that make the city a desirable place in which to live.

To this aim, the City established a Design Review Committee. This panel is charged with the review of proposals for restoration and new development in these key districts and in other areas as directed by the City Council. Proposed projects submitted to the Design Review Committee must meet the development standards outlined in these guidelines.

**I.C Regulation and applicability.**

The following Design Guidelines illustrate the architectural features that are important to maintaining the character of the business district and to establishing an appropriate identity for the town as it grows. The guidelines are to be applied to all new construction, reconstruction, and changes of use from residential to commercial. Except for multifamily residences, no residential structure that is continuing to be residential must conform to these guidelines.

**I.D. Unifying Elements.**

Each district of the city should reflect an aspect of this city's personality. In the comp plan it's called the "small town character." It is the character of the city that says it's friendly and people are welcome as they are, that we're proud of our history, and that we care about one another. This character needs to be shown in each development through the choice of color, style, landscape, and roof form. Signs can also communicate a town's personality and the type of sign used for each development should be designed with care to further the city's character. The comprehensive plan describes the city's small town attributes as follows:

**Policy 3.5.1**

*The city's "small-town attributes" include at least the following: walkability across town, walkability to food, pharmacy, and businesses, residential porches facing street fronts, and large picture windows in businesses along Main Street. Providing pedestrian connections should be required as part of all development because these connections will promote pedestrian traffic to the business community.*

Suggested sign fonts:

Copperplate  
Gothic  
Heavy  
ATF

ABCDEFGHIJKLMNOPQRSTUVWXYZ  
XYZ1234567890&\$?!

E1 E1 E1 E1 E1  
21-60-CN 21-48-CN 21-36-CN 21-30-CN 21-24-CN  
E1 E1 E1  
21-18-CN 21-14-CN 21-12-CN

abcdefghijklmnopqrstuvwxy  
ABCDEFGHIJKLMNPOQRSTUVWXYZ  
1234567890 &?!\$%(',:)

E1 a E1 a E1 a Eal Eal Eal  
60-98-CN 60-86-L 60-75-CN 60-72-L 60-60-CN 60-60-L 60-48-CLN 60-42-CLN 60-24-CLN

Playbill  
Stephenson Blake



Examples of Appropriate Signage Lettering Styles

Quentin

ABCDEFGHIJKLMNOPQRSTUVWXYZ  
1234567890 &?!\$%(',:)

E1 E1  
117-72-CN 117-48-CN

Rockwell  
Condensed 414  
Monotype

abcdefghijklmnopqrstuvwxy  
ABCDEFGHIJKLMNPOQRSTUVWXYZ  
1234567890 &?!\$%(',:)

Eal Eal Eal  
327-48-CLN 327-38-CLN 327-24-CLN

Rockwell Shadow  
Monotype

ABCDEFGHIJKLMNOPQRSTUVWXYZ  
1234567890 &?!\$%(',:)

E1 E1  
411-48-CN 411-38-CN



Examples of Appropriate Signage Lettering Styles

## II. Design Approach

### II.A Introduction.

The character of Buckley's buildings is based on the city's development as a working community with ties to farming, lumber and railroad. Its commercial buildings are well constructed and are interesting not because of elaborate ornamentation, but for their history, craftsmanship and proportion. Buildings outside of town have always been utilitarian structures that stress strong, simple, functional forms over decoration. It is not the intent of these guidelines to introduce a clever theme or arbitrary style to the city, but rather to promote the honest restoration of existing structures and the construction of new ones in a manner that is in keeping with the history and spirit of the community.



### II.B Purpose.

To coordinate future development the City of Buckley has, through ordinance, established a Design Review Committee made up of three City Council members. Any exterior construction, reconstruction or remodeling of any structure, excluding single family dwellings, within the Design Review districts is reviewed by this committee.

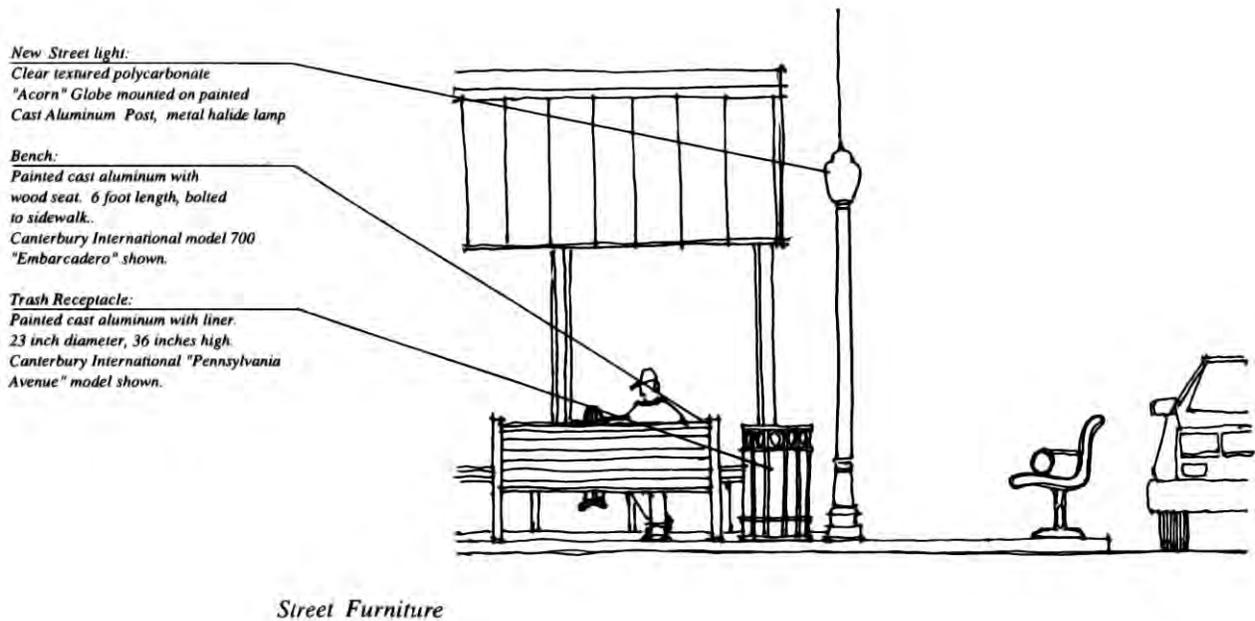
Districts established by the City Council must be reviewed and recommended for approval by the Design Review Committee as a part of the building permit process.

### II.C Regulation & Applicability.

#### 1. General regulations.

General regulations are found in the Buckley Municipal Code (BMC). These regulations include and are not limited to the following:

- a. A building height of 35 feet (BMC 19.20.010(2)); these guidelines may specify the minimum and maximum number or height of stories for each design area;
- b. Setbacks and lot coverage are specified in each zone's section (Sections 19.20.050 through 100, BMC);
- c. Permitted and conditional uses are listed in BMC 19.20.130;
- d. Parking requirements are addressed in Chapter 19.28 BMC;
- e. Landscaping is addressed in Chapter 19.29 BMC; and
- f. Signs are addressed in Chapter 19.30 BMC; signs are also addressed in these guidelines.
- g. Permitting is addressed in other sections of the Buckley Municipal Code and will apply to each application.



## 2. Design review.

Applications within the design review areas for commercial, multifamily, and mixed uses must be reviewed by the design review committee (DRC) before issuance of a building permit. Other development that is subject to design review will include major repairs as described in the code chapters requiring design review (BMC 19.50 and 19.51). These chapters define Buckley's Design Review Districts as defined by City ordinance, and , which can be described as follows:

- a. Downtown Review District.  
Includes Main Street and is roughly bounded by the foothills trail to the east side of the alley between A and B streets, and from Park to Mason avenues.
- b. Highway 410 Review District.  
All property abutting the highway within the city limits in GC, CC, NMU, LI and P zones, and property in zones adjacent to these zones. This area includes all properties abutting SR 410 in the CC, GC, NMU, LI, and P zones, and properties adjacent to these properties with development visible from SR 410.
- c. Gateway District. The Gateway District is roughly bounded by SR 410 and River Avenue from SR 165 to Park Avenue.

## 3. Signs.

Signs are regulated both by this document and Chapter 19.30 BMC. The regulations common to each district are as follows:

- a. All signs shall comply with Chapter 19.30 BMC and these design guidelines.
- b. Sign colors shall be compatible with the colors and materials of the building façade and as described in these guidelines.
- c. Signs shall not obscure key architectural elements, doors, or windows.
- d. Simple shapes are preferred over complex geometries.
- e. Signs shall be professionally designed and constructed with high-quality materials.
- f. Signs on adjacent storefronts shall be coordinated in height and proportion and designed to unify the street elevation.



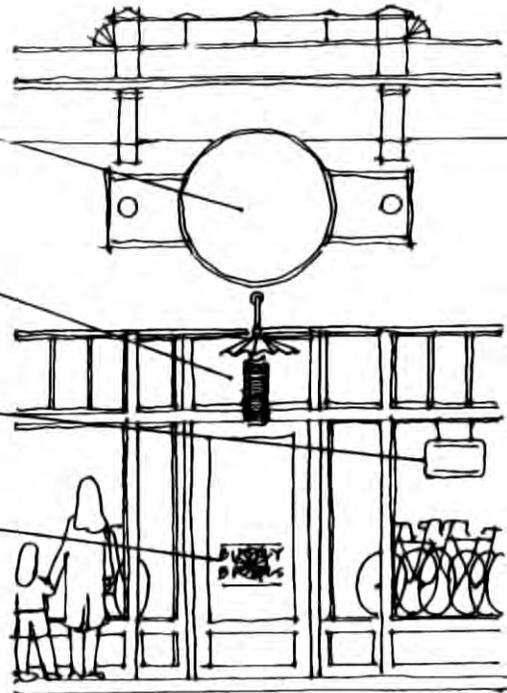
- g. Sign structure shall be designed to complement the sign and not clash with or overwhelm it. Attachments to building materials shall not permanently damage any architectural ornament or feature and shall avoid damaging bricks or masonry.*

*Flush mount painted Wall Signage:  
Gloss enamel on wood or metal.  
Unlit (shown) or externally lit.  
Vehicular scale.*

*"Blade" Sign extending perpendicular  
from building face. Gloss enamel on  
wood or metal. Externally lit from above  
(shown) or side, or unlit.  
Pedestrian scale.*

*Internal window signage. May be  
internally lit, neon or unlit, depending on  
age of building with approval of ARB.*

*Painted window signage. Opaque gold  
leaf or colored messages and graphics.  
Pedestrian scale.*



#### 4. Submittal Requirements & Review Criteria.

Submittals to the Design Review Committee for proposed new construction and remodeling projects within these zones must be made in the following format. Incomplete submittals may not be reviewed.

- a. The review body is either the Design Review Committee (DRC), or for minor projects, such as signs in the SR 410 district, the city planner.
- b. The applicant should show substantial compliance with appropriate elements of the design guidelines.
- c. General Requirements

All drawings (each sheet) must contain the following information:

1. Project Name, file number of associated site plan review, if applicable;
2. Project Address and parcel number;
3. Date, and an area for revision dates; and
4. Name, address, e-mail address, contact, and phone number of firm responsible for drawings.
5. North Arrow
6. Drawing scale: site development plan to be drawn at 1"=20' or 1"= 30'-0.
7. Existing buildings to be removed or retained.
8. Major landscape features existing and proposed.
9. Existing and proposed topographic contours at 2'-0" intervals.
10. Existing streets and roads on or bordering property, including curb lines and sidewalks.
11. Existing buildings and features within 100 feet of subject property lines.
12. Exterior lighting location and types including mounting heights.
13. Exterior sign locations and types.

## 5. Application Information.

Please indicate the following information on the design review application or on a the elevation drawings:

- a. Area of proposed structure in square feet by floor.
- b. Lot coverage of structure (please state lot size in square feet).
- c. Lot coverage by impervious surfaces.
- d. Building height (please indicate the height of the highest part of the highest gable and the height as measured in accordance with the building code).
- e. Number and location of parking spaces.
- f. Area, location, and depth of landscaping.
- g. Landscaping Plan.



A landscape plan meeting the requirements of BMC 19.29 should be submitted with the site plan review, but it may be deferred until design review or toward the end of construction, if the DRC has sufficient information about the general design and intent of the landscape design.

1. Extent and location of all plant materials and other landscape features. Label individual plants or include a planting legend.
  2. Proposed planting materials drawn to scale at mature sizes and indicating spacing. Indicate mature heights.
  3. Species and size of existing planting materials to remain.
  4. Location of water outlets or schematic irrigation system plan.
- h. Exterior Building Elevations:
1. Architectural drawings at 1/8"=1" or 1/4"=1" Building elevations are to be labeled with directional views (north, south, etc) rather than "front" or "right."
  2. Indicate doors and windows, architectural details, materials and finishes.
  3. Note existing and finished grades, base elevation and elevation of highest projection of building.
  4. Provide color and exterior material samples of the proposed color pallet.
  5. ~~Indicate sign to be located on building.~~ Illustrate on the plans the likely location(s) of any sign, whether on the building (façade) or on the site (monument)
- i. Architectural Details:
1. Details drawn to scale at appropriate sizes for all ornament, exposed structural systems and other details as required to describe proposal.
  2. The Design Review Committee may require addition drawings of submittals for specific projects. Additional submittals are required for building and fire department approvals.

### III. Downtown review district

#### III.A Introduction.

The downtown review district contains a mixture of commercial, public and residential properties. Its buildings include the well-defined commercial core on Main Street.



#### III.B Purpose

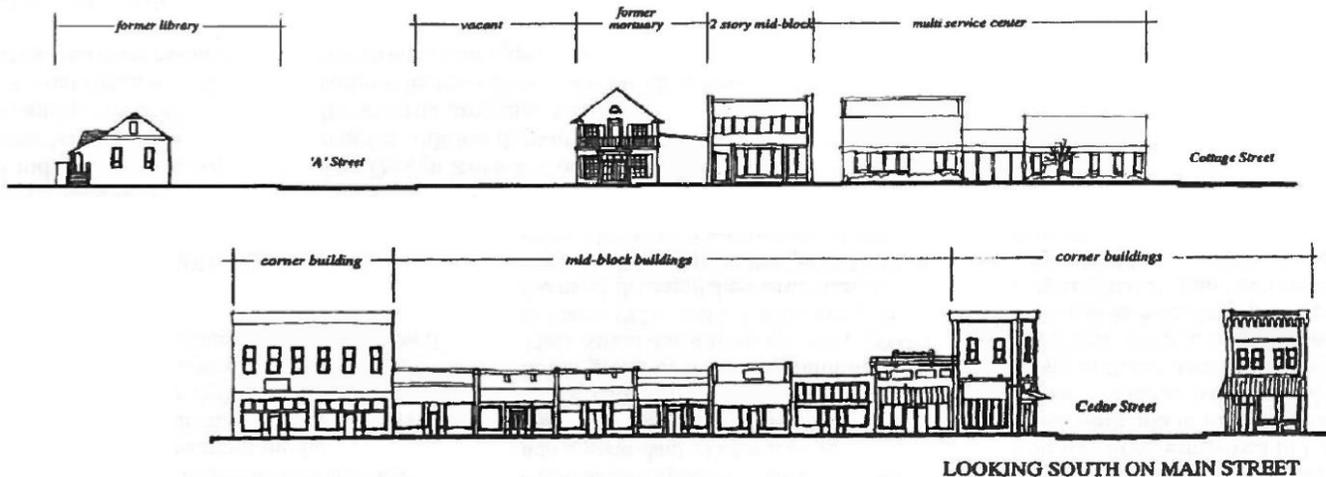
These redevelopment *design* guidelines apply only to commercial and public structures. Single family residences are not subject to review unless they are to be converted to commercial use.

#### III.C Regulations

In the restoration of existing buildings downtown or construction of new structures, the elements that are most important to maintaining the architectural character of building on Main Street, as defined by these guidelines or required by the Design Review Committee, shall be maintained or restored.

*The historic district contains two zones, which are to be regarded differently. While all historic buildings should be restored as much as possible, the emphasis for the city is to maintain the historic look of the downtown area. This would include colors, signs (including the lack of flashing neon signs), and ornamentation.*

*The area surrounding the historic core is called the Neighborhood Mixed Use zone (NMU), and is designed to serve as a buffer zone between the commercial and residential areas. As a buffer, the*

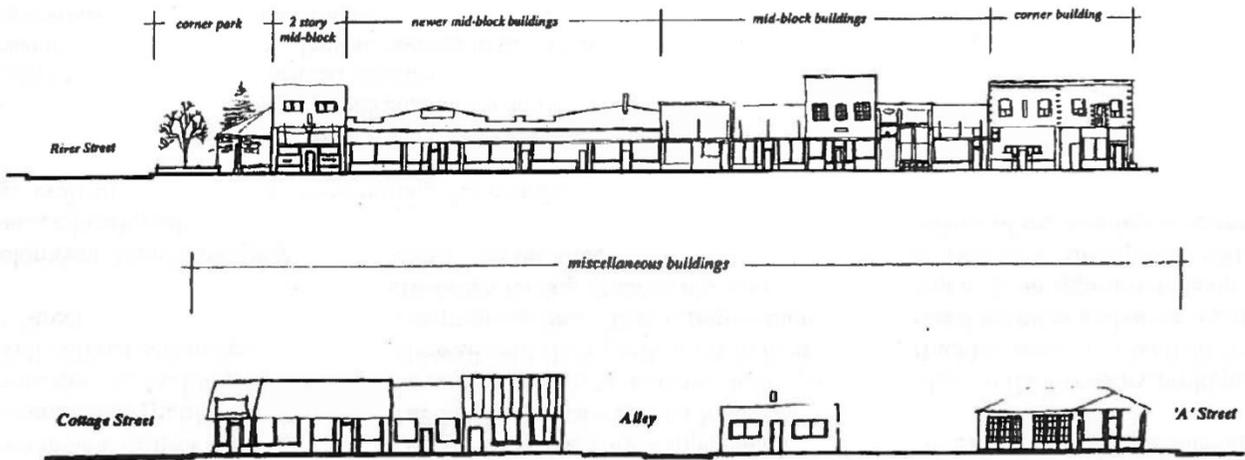


*architecture should reflect a portion of the historic requirements, but also reflect the designs of a single family neighborhood. The NMU zone currently allows single family residences as a permitted use, commercial uses, and commercial/residential uses.*

#### 1. Main Street Buildings.

The buildings of Main Street are of several types differing in age, style and importance to the City's appearance. The buildings that make up Main Street's core have no setbacks from the sidewalk and share party walls at their side property lines. This is important to creating a feeling of enclosure and identity on the street.

Main Street buildings are divided into types: corner buildings, mid-block buildings, and miscellaneous buildings. In the diagram above, you can see buildings on the south side of Main Street. Please note the characteristics on the corners and mid-block buildings.



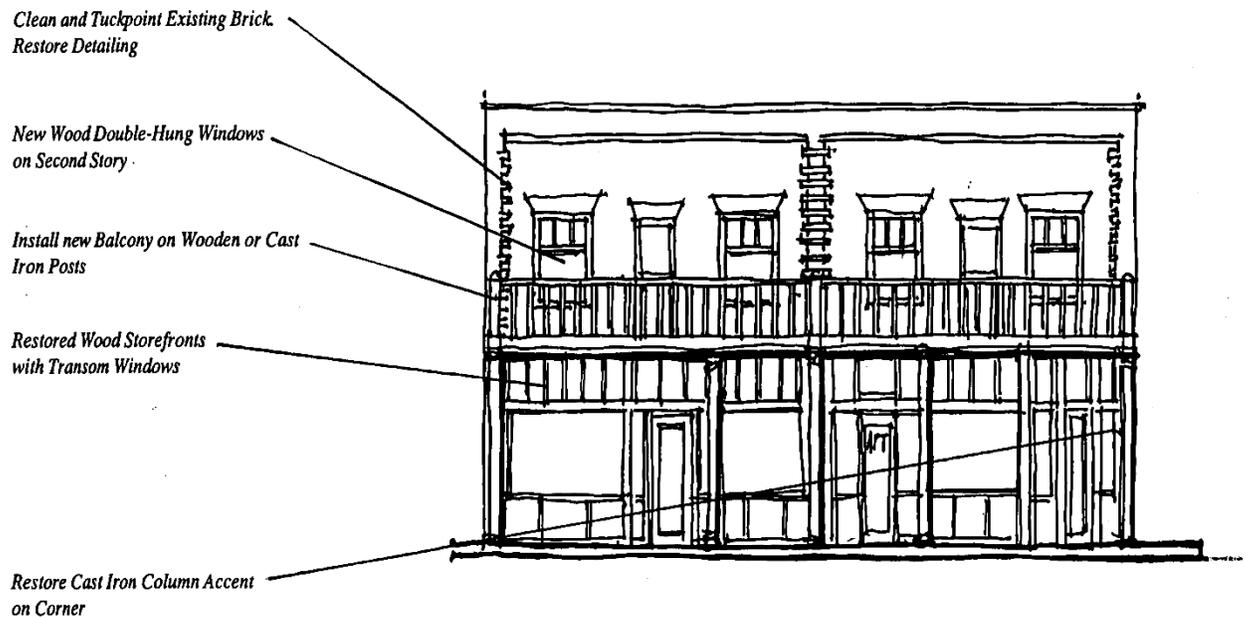
LOOKING NORTH ON MAIN STREET

## 2. Corner Buildings.

Some of the oldest remaining downtown structures are at the ends of each block. Typically they are unreinforced stone and brick, two stories tall with commercial tenants at street level and office or residential occupancies upstairs. These buildings add a great deal of character to Buckley, providing visual accents to define each major intersection. This oldest group of remaining buildings on Main Street dates from the early 1890's to about 1920, and contains many of the most distinguished structures in town. Buildings from this period reflect many elements characteristic of turn-of-the-century architecture: traditional stone and masonry work, cast iron storefronts and stamped metal cornices.

Most of the two story buildings in Buckley were built with elaborate metal cornices and other ornamentation that adds an appropriate level of finish to their tops. Earthquakes and the effects of the weather have removed most of these details, but evidence of their overall architectural effect is visible in historic photos of Main Street and in commercial districts of similar age in other cities.





*Example: Corner Building: 780-790 Main*

In typical buildings of this period, cast iron was often employed to imitate stone work and to form structural elements such as lintels or columns. Many of these elements remain but have been covered or disguised by non-conforming materials. Other cast iron elements have often been removed. The corner buildings include the following features:

- More than one story height, usually with commercial spaces on street level and offices or residential space above.
- Traditional brick and stone construction, often with elaborate detailing.
- Flat roofs with dominate cornices.
- Cast iron storefronts or wood storefronts.
- Wood sash double-hung windows.

These buildings are particularly important in providing definition to the corners and the end of blocks. It is their dominance at intersections that gives Buckley much of its character.

**3. Mid-Block Buildings.**

Between these larger structures are two groups of one-story commercial buildings, the groups being identified by age and type of construction. They are all similar in height, scale and detailing, and serve to tie the blocks together.

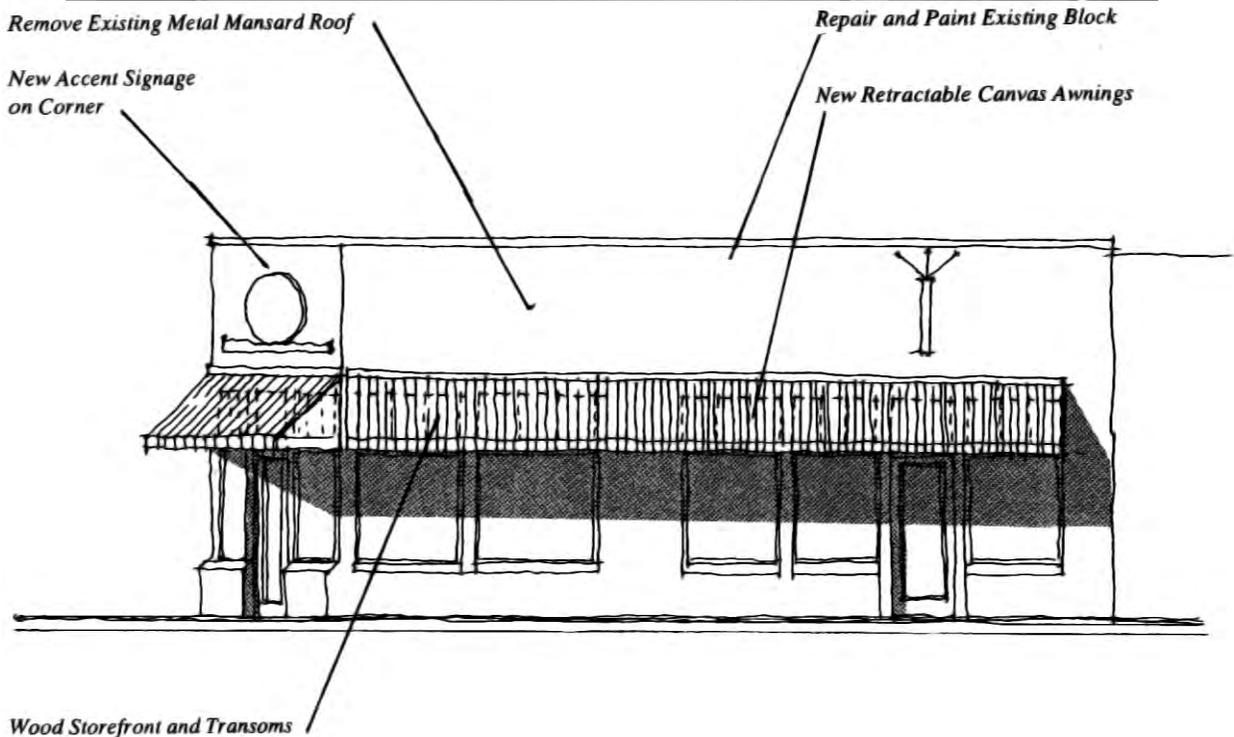
Many of these structures are as old as the corner buildings, but are more modest in scale and detailing; while not the first structures you notice on a walk down Main Street, they provide much of the detail and pedestrian scale interest that can give Buckley's shops their individuality. The mid block buildings share these elements:

- Typical twenty-five feet wide, brick construction.
- Usually a wood framed cornice on brackets was applied over the storefronts.
- One-story, commercial or professional buildings.
- Structure is less ornamental, with most of the detailing contained in storefronts and windows.

- e. Storefront glazing is maximized to show off merchandise. Infill below the windows is usually wood, as are sash members doors.

Some of the mid-block buildings were constructed more recently and constructed of poured concrete or concrete block rather than brick. They lack the detail of the older buildings both in structural detailing and storefront interest. They do, however, provide visual continuity to Main Street. These buildings were built with economy in mind and are the second generation of structures in Buckley, built after the fire of 1925. They should be viewed as “background” buildings taking a backseat to their older, more ornate neighbors, and should not be ornamented to imitate them. It should, however, be noted that their infill function makes them an important part of downtown.

- a. Single story, commercial occupancies.
- b. Concrete or concrete block construction.
- c. Very simple lines with no ornamentation.
- d. Metal storefronts and doors that have replaced the original wood systems.



#### 4. Miscellaneous Commercial Buildings.

The district contains several miscellaneous buildings with more individual characteristics. They exhibit a variety of roof forms, more variation in materials and vary greatly in age and use.

Because yards and setbacks are introduced for parking, landscaping or service, these structures serve as a natural buffer and transition to the residential neighborhoods that surround the commercial center.

- a. Former Methodist Church and Mortuary, 873 Main Street.

A dignified wood frame structure which was moved to its present site many years ago. Its history and formality earn it a place on Main Street.

- b. Former Gas station at “A” Street and Main, 818 Main Street.

This type of gas station dates back to the forties and is becoming rare. The scale of the service bays, windows and roof forms make it good transition to the residential neighborhoods to the east.

- c. Café, Offices and Laundromat on north side of Main, between 828 and 840 Main Street. These buildings provide some mid-block infill but are of such different materials from all other buildings in town that they should be dealt with individually as “background structures”.

- d. City Multi-Purpose Building, 811 Main Street. On the site of the Old Buckley Hotel, this one story brick and wood public building was built in the seventies and was an attempt to reflect Buckley’s rural setting. It is an example of roof forms, material, building siting and scale that do not contribute to the traditional personality of Main Street.

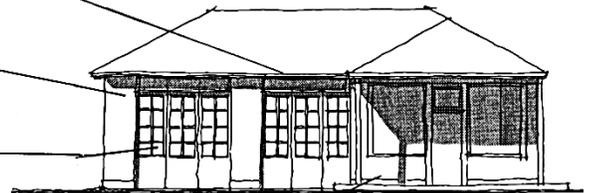


*Signage Locations over Service Bays*

*Repaint and Restore Structure to Original Appearance*

*Restore Original Service Bay Doors*

*Creatively Reuse Existing Architectural Elements for New Purposes. Canopy Becomes Outdoor Seating area or Retail Display Space*



The architectural character of each shall be identified and maintained, with renovations planned to be true to the original construction, and subject to Design Review Committee approval.

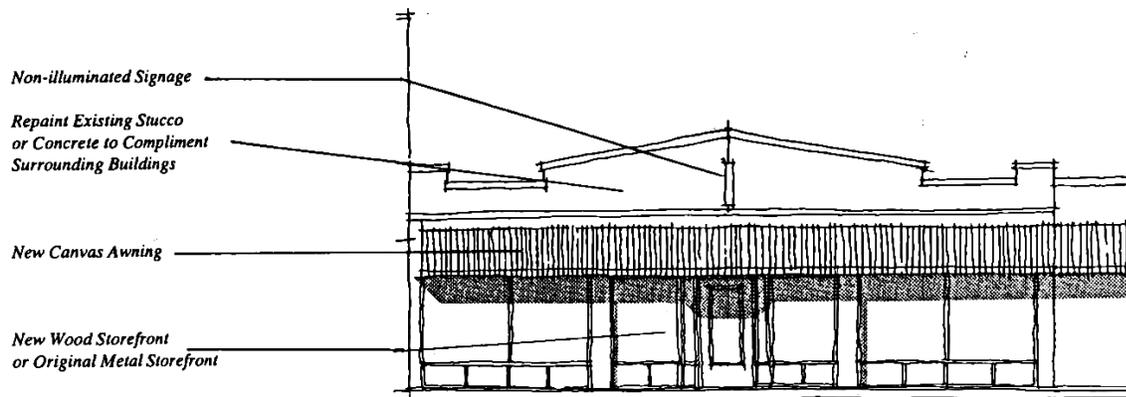
Buildings that lack strong character shall be renovated to compliment the rest of the Downtown District without resorting to gimmicks or false ornamentation.

- e. New Commercial Buildings in the NMU Zone. Construction, reconstruction, in-fill, or conversions from residential to commercial uses in the NMU zone surrounding the historic district should use the following standards:
  - i. The use should be set back from the property in conformance with the zoning ordinance in effect at the time the commercial use is to be established.
  - ii. The area between the sidewalk and the building should be landscaped or decorated.
  - iii. New construction should have no more than two floors.
  - iv. One-story buildings should have pitched roofs like a house.
  - v. Two-story buildings may have pitched roofs.



### III.D. Architectural Design.

Restoration and renovation of Downtown Review District buildings *in the HC zone* shall be carried out in accordance with the following guidelines and as directed by the Design Review Committee. Restoration or redevelopment in the NMU zones may use a different set of standards.



*Example: Newer Mid-Block Building*

#### 1. Masonry.

Non-original materials such as wood, metal or plastics are not acceptable substitute materials for original masonry walls.

- b. Clean brick and stone with water and non abrasive cleaners that will not erode or change the surface of the material. Sandblasting is not acceptable.
- c. Repaint brick and stone joints to match original grout profile. Repair cracks and replace non-conforming masonry work or other non-masonry materials with brick or stone that matches the color and dimensions of original materials
- d. Most brickwork in Buckley was originally unpainted. Whenever possible, unpainted conditions should be restored. Use of high quality, low sheen sealers is acceptable. If repairs to brickwork are extensive or matching materials are not available, painting of brick may be an acceptable alternative.
- e. Perform seismic bracing without damaging or destroying original materials. The introduction of bolts and new structural supports on the building's exterior should be avoided or carefully planned.
- f. Repair masonry cornices to original profiles. Substitute materials may be acceptable to achieve original visual appearances if structural or other construction constraints do not allow reconstruction in masonry.

#### 2. Concrete or Stucco Exteriors:

Patch and repair as required. Remove any non-original materials such as wood or metal siding. Repaint to compliment natural brick structures.



**3. Wood Storefront and Detailing:**

Restore wood cornices and storefronts to original condition using historic photographs as a guide. Much of the character of these buildings lies in the quality and proportions of the carpentry work and much pedestrian interest can be created through proper restoration.

**4. Wood Window**

- a. Windows of this building type are typically taller than they are wide, in a 2:1 or 3:1 ratio. Upper transom panels are broken into individual lites that are vertically oriented. These proportions as well as the size and thickness of sash and mullion members are to be maintained or restored to original appearance.
- b. Newer, energy efficient, types may be considered by the DRC when constructed in a style that compliments the intent of these guidelines.



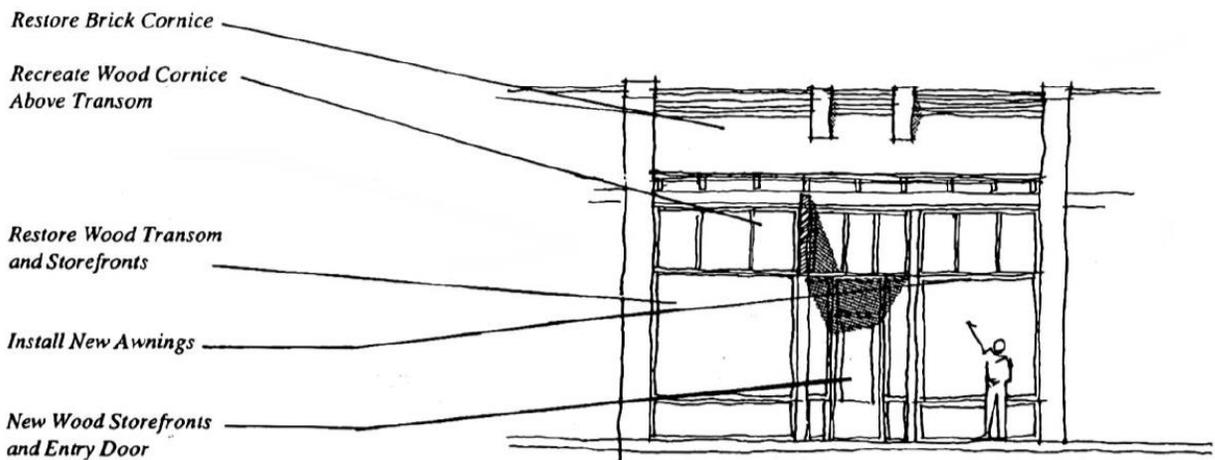
**5. Metal Cornices and Ornament.**

Repair or replace building cornice work with new materials, basing profiles on photographic evidence available. Original stamped aluminum or fiberglass castings made from remains of any existing cornice fragments. Paint restored cornices and ornament to match original, basing color selection on shades and tones visible in photos, and on evidence of color preferences known from similar buildings.



**6. Cast Iron Structure and Storefronts:**

Restore or replace all cast iron work to match original profiles. Fiberglass or aluminum castings are acceptable substitutions for cast iron that is missing or beyond repair. Paint new work to replicate historic conditions, as based on photographic evidence and similar built examples from adjacent structures or other cities.



### III.E New Construction and Unifying Elements

New structures built within the Downtown Review District shall be constructed to compliment existing buildings, but should not attempt to imitate or literally copy them. Setbacks, roof forms, building heights and general massing shall be in keeping with the existing structures on either side of the proposed site.

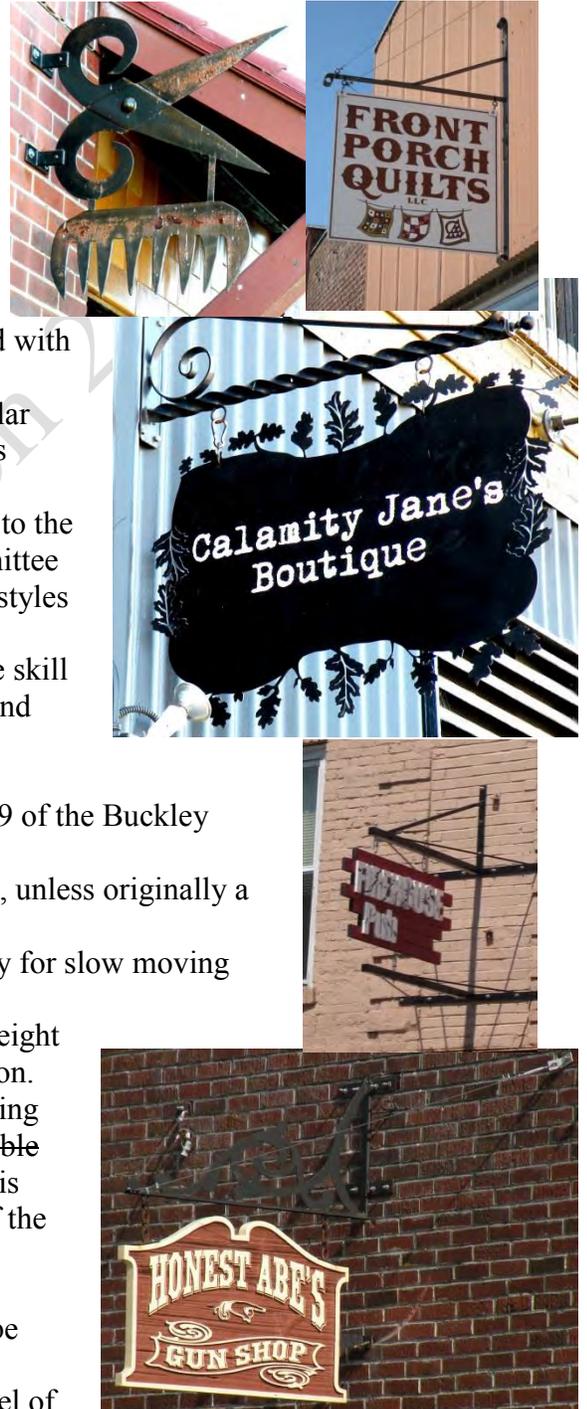
New buildings constructed on Main Street shall be designed to reinforce the traditional pattern of two story corner structures and one story mid-block buildings.

Doors, windows, details and ornamentation shall be of similar materials, scale and proportion to those elements found on existing buildings and described elsewhere in these guidelines.

#### 1. Signs:

Signs in the Downtown Review District are to be designed with pedestrians in mind. The size, scale, color and lettering of messages shall be appropriate to walking and slow vehicular speeds, and be reminiscent of original Main Street signs as found in photographic documents.

- a. All new signs and all changes in existing signs visible to the public shall be submitted to the Design Review Committee (DRC) for approval. Signs shall be reminiscent of the styles and materials available at the time the building was constructed. The DRC shall take into consideration the skill and technical quality of signs submitted for approval and may rescind approval for any sign not executed in accordance with materials submitted for review.
- b. Sign area shall meet the thresholds specified in Title 19 of the Buckley Municipal Code.
- c. Signs shall not cover architectural ornamental features, unless originally a feature of the building.
- d. Signs and individual letters shall be sized appropriately for slow moving traffic and pedestrians.
- e. Signs on adjacent storefronts shall be coordinated in height and proportion and designed to unify the street elevation.
- f. Projecting signs and signs placed flat against the building wall are encouraged. Projecting signs shall be compatible with consistent with the period of building to which it is affixed, and shall be consistent with the time period of the building.
- g. The restoration and preservation of historic wall and architectural signs is encouraged. No new signs shall be installed or painted in place of historic signs.
- h. Signs shall be professionally lettered and display a level of craftsmanship appropriate to the downtown district. Lettering shall be of a traditional block



or curvilinear style which is easy to read and *similar to* the style of building. Generally, no more than two different lettering styles should be used on one sign. Examples of acceptable lettering fonts are included in these guidelines.

- i. Construction shall be of metal, wood or other durable material as approved by the DRC and appropriate to the building on which the sign is installed.
- j. Sign colors shall be appropriate to the era in which the building was constructed and in all cases be *consistent with* the predominantly red brick facades of downtown buildings.
- k. Signs in the downtown district shall be unlighted or lighted from external sources. Neon signs may be acceptable to the DRC if custom-designed to be *similar to* the building's historic or architectural features. *Neon signs shall not flash, blink, move, twirl, or manipulate lettering.*
- l. Sign structure shall be designed to complement the sign, not clash or overwhelm it. Attachments to building materials shall not permanently damage any architectural ornament or feature, and shall avoid damaging bricks or masonry.

## 2. Awnings.

- a. Awnings have been a traditional addition to the facades of buildings downtown and shall be encouraged as a unifying exterior feature.
- b. All awnings shall be compatible with complement the historic character of the buildings on which they are placed and shall be based in design upon historic counterparts. They shall reflect the architectural style and form of the building and shall be, or appear to be, retractable in keeping with historic precedent. They shall be attached to the building in a manner that does not permanently damage the structure or obscure significant architectural features.
- c. Construction shall be of canvas like materials in traditional textures and sheen. Back-lighted or translucent awnings are not allowed.
- d. Awnings shall be in color and/or patterns which complement the building and have basis in the historical record



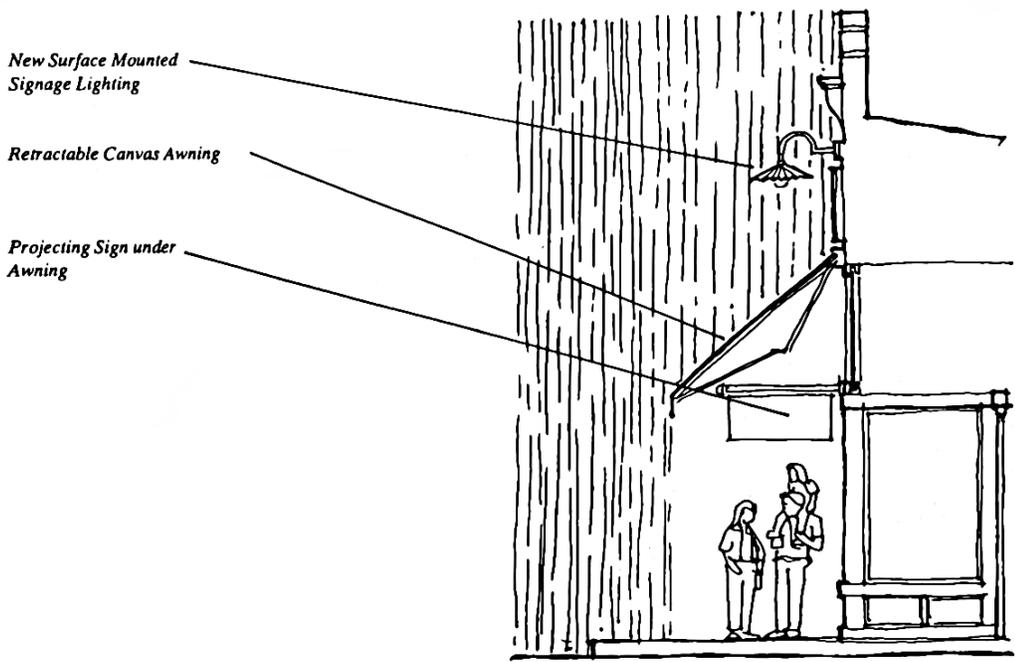
## 3. Building Color.

Color for existing buildings in the Downtown Review District shall approximate the original colors of materials and finishes. Materials may be scraped to provide evidence of former schemes, or written or anecdotal evidence used to establish an authentic color palate. Since color is subjective, however, the original color may have been inappropriate and painted over. Discretion should therefore be used in the selection of any new color scheme. Several national paint companies have introduced colors which have historic origins. Review of historic colors appropriate to the buildings age can be a good method of selecting a scheme. Any colors selected should be judged for their appropriateness to use in the City of Buckley.

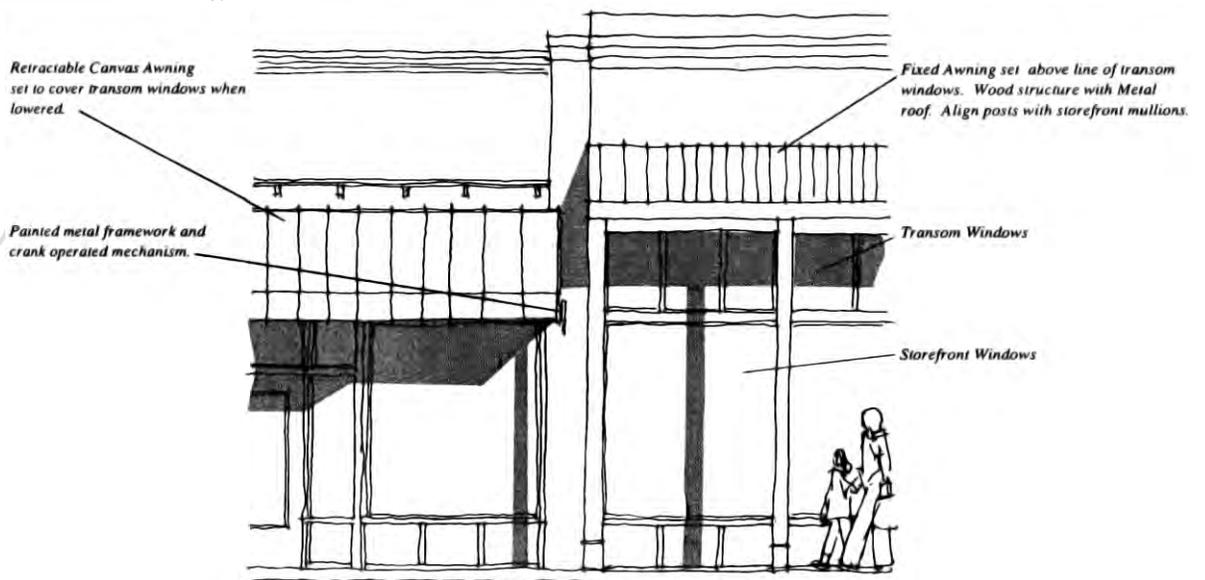
All color schemes shall be reviewed and approved by the DRC prior to application. The DRC may require sample applications of paint colors on site before final approval. Building colors shall contribute to the architectural character of the building and be considered in combination with signs, awnings and other elements.

Original building colors shall be considered for any existing building renovation. For new construction, colors should be chosen that are muted, earth tones and work well with the overall red brick color of most downtown buildings. Color schemes employing many colors or particularly bright colors shall be avoided.

Color shall be selected to emphasize building form and to highlight major features. Surfaces which were not painted originally shall not be painted unless specifically approved by the DRC.



*Awnings*



*Awnings*

PC recommendation 20160711

**IV. Rainier gateway design district**

**IV.A Introduction.**

These design guidelines are intended to guide land use and site development in the Rainier Gateway Subarea. New developments and renovation of existing buildings and properties serving current and new uses are required to comply with these design guidelines as interpreted by the DRC as a part of the permit process. The City believes that adherence to these design guidelines will lead to a more successful and cohesive Rainier Gateway Subarea.

**IV.B Purpose.**

Buckley's Rainier Gateway Subarea presents a unique opportunity for the community to connect the development in the SR 410 Corridor with the revitalization of the historic Downtown. The design of the Rainier Gateway Subarea should reflect the community's desire to preserve open space and views of Mount Rainier while transitioning from the auto-dependent SR 410 to the more pedestrian-friendly historic Downtown while striving to build on the non-motorized connections created by the Foothills Trail.



**IV.C Regulations.**

Development in the Rainier Gateway design district should connect the western commercial SR 410 district with the downtown district. These connections should be visual as well as by use.

**1. Pedestrian Connections.**

- a. Developers shall provide connections to adjoining neighborhoods, access to the Foothills Trail, and provide pedestrian and bike paths for their development.
- b. Where pedestrian walkways cross streets or internal vehicular routes, they shall be distinguished by a change in paving material, color, and texture.
- c. Crosswalks shall be clearly striped to improve visibility and safety of the pedestrian.

**2. Foothills Trail Connections.**

- a. Wherever possible, separate bicycle and pedestrian paths should be provided. If it is not feasible, additional width, signs and pavement markings should be used to lessen conflicts between Foothills Trail users.
- b. Provide signs with typical notices for multimodal users to



accommodate pedestrians, runners, bicyclists, and skaters.



### 3. Bicycle Amenities.

- a. All new development shall provide adequate bicycle infrastructure, such as sidewalks, crosswalks, bike paths, and bike racks.
- b. Bicycle parking facilities shall be conveniently located to either in the street right of way, along the Foothills Trail or at the main entrances to buildings. Bicycle parking facilities should be no farther away than the closest parking space. The location of the bicycle parking facilities and subsequent parking should not interfere with pedestrian passage.
- c. Where bicycle parking facilities are provided, they shall be stationary racks that support the bicycle with at least one point to which the user can lock the bicycle and one wheel and frame with a high security U-shaped lock or cable lock. Rail-type, Inverted U, and Cora are recommended options.
- d. Original designs for bicycle parking facilities that reflect local character are encouraged. Bicycle parking facilities should also be weatherproofed and/or covered whenever possible.
- e. Bicycle parking facilities shall be located where they are visible from street sidewalks or building entrances, so that sufficient security from theft and damage is provided.
- f. Residential bicycle parking facilities shall be located in a secure location, either indoors or in a locked, covered outdoor cage.

### 4. Open Space.

Plazas and open space shall be provided to link the Foothills Trail with commercial areas.

- a. A combination of four or more site furnishings shall be provided in open space areas or pedestrian walkways. Site furnishings include but are not limited to:  
Trash and recycling receptacles (required as one of the four site furnishings).
- b. Benches/seating.
- c. Tables.
- d. Bicycle racks.
- e. Drinking fountains.
- f. Pedestrian scaled lighting (other than streetlights in the public right-of-way).
- g. Public art.

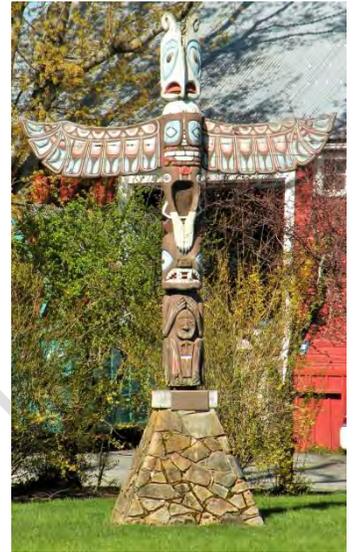


- h. Container plants.
- i. Where possible, seating areas shall be oriented to the Foothills Trail and/or toward views of Mount Rainier.

**5. Design.**

Landscape design between the Foothills Trail and SR 410 shall use native plantings as much as possible. A list of native plants is available at City Hall. Where native plants are not used, the designer shall indicate why the non-native species has been selected and list its benefits over a similar native plant.

- a. Plantings shall be selected to support their intended use. Where recreational use of open space is provided adjacent to the Foothills Trail, plant selection should consider the impacts of this use on nearby shrubs and groundcover.
- b. Open spaces and plazas shall be framed with trees to shade and define public spaces.
- c. Existing large trees shall be retained wherever possible to provide shelter for gathering spaces. These trees serve as symbols of Buckley's logging heritage.



**6. Signs.**

All signs shall comply with BMC §19.30 and these design guidelines.

- a. Sign colors shall be compatible with the colors and materials of the building facade.
- b. Signs shall not obscure key architectural elements, doors, or windows.
- c. Simple overall shapes are preferred over complex geometries.
- d. Signs shall be professionally designed and constructed with high-quality materials.



**7. Lighting.**

- a. Lighting along River Road and the Foothills Trail should help users feel safe and comfortable, while still preserving the feeling of rural open space in the corridor.
- b. All building entrances shall be illuminated. Commercial buildings and landscaping can be illuminated indirectly by concealing light fixtures within buildings and landscaping to highlight attractive features and avoid light intrusion into neighboring properties.
- c. Lighting fixtures should satisfy dark sky requirements and use minimal power.
- d. Lighting fixtures located on building should be concealed or integrated into the overall design of the project. The light source should be hidden from direct pedestrian or motorist view.



**8. Public Art.**

Public art should serve as a landmark and establish a gateway at the intersection of SR 410 and Main Street.

- a. The Rainier Gateway Subarea Design Guidelines pertaining to style and materials should inform public art themes.
- b. Natural materials such as wood and stone are highly recommended. Other materials that reflect Buckley's historic character are encouraged, such as iron and Wilkeson sandstone. Modern materials such as glass and plastics are discouraged.
- c. Public art should recall Buckley's heritage, accentuate the Foothills Trail, and/or highlight Buckley's natural surroundings.
- d. Functional public art, including but not limited to benches, shelters, bike racks, or children's play equipment, is encouraged.

**9. Other Elements.**

- a. Signs shall be provided to guide trail users to public restrooms, commercial uses, and points of interest.
- b. Foothills Trail amenities and features should be aesthetically pleasing, encourage use, and discourage illegal behavior.
- c. Public bathrooms and picnic structures should follow the materials design guidelines specified for Rainier Gateway Subarea buildings.
- d. Public refuse receptacles in commercial sites shall consist of low-saturation colors and be simple in design.
- e. Pavement colors shall be compatible with other pavements in Buckley. Commercial open space and plazas can use cool colors and varied saturation, degree of darkness, and shapes.



**IV.D Site Design**

Projects within the Rainier Gateway Review District shall be designed in compliance with all City of Buckley planning and development requirements. In addition, projects subject to review shall include:

**1. Orientation:**

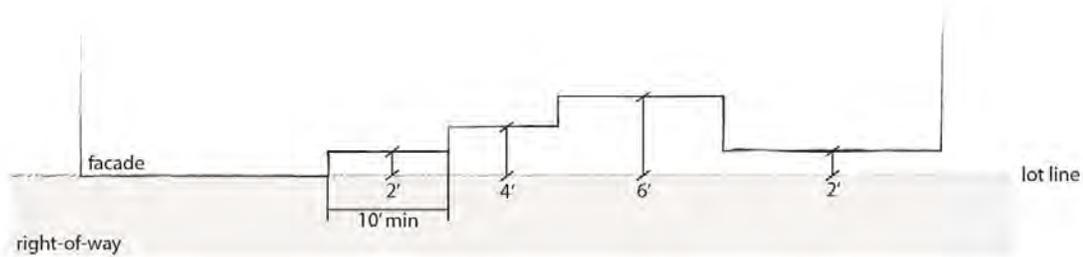
- a. Buildings shall be oriented to the street, with a secondary entrance facing the Foothills Trail to encourage pedestrian activity and define common open space.
- b. Buildings shall define edges of public space and create a comfortable, pedestrian-scaled environment.
- c. Buildings located on corner lots shall be designed to positively define and frame both streets they front.

**2. Setbacks.**

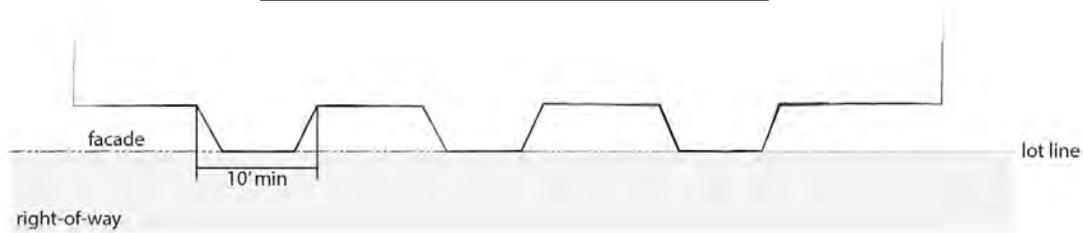
Building setbacks shall be the same as specified in the individual land use districts, except for the following:

- a. Where a commercial uses is located on the first floor, the front setback may be reduced to zero.
- b. Where minimum requirements for side setbacks exist, these may be reduced to zero only where buildings are attached.
- c. The maximum front setback from River Road shall be ten feet, where plazas, landscaping, benches, bicycle parking, and other pedestrian and trail-user amenities are provided. Where pedestrian amenities are not provided, and the proposal is for a commercial use, buildings shall be located at the front lot line, in order to create a distinct street edge and foster a more pedestrian oriented environment.

- d. A maximum five foot side setback shall exist where the following conditions occur on the opposite side lot line:
  - i. Where parking is located on the side of the building.
  - ii. Where public open space is located on the side of the building.
  - iii. Where additional width is required to accommodate pedestrian walkway between



**Example: Horizontal Modulation**



the Foothills Trail or River Road and the primary entrance.

- e. Where buildings are setback from the Foothills Trail along the rear frontage, the space shall contain public and trail-user amenities. Outdoor dining and seating areas may be located within the rear setback.

### 3. Building Entrance:

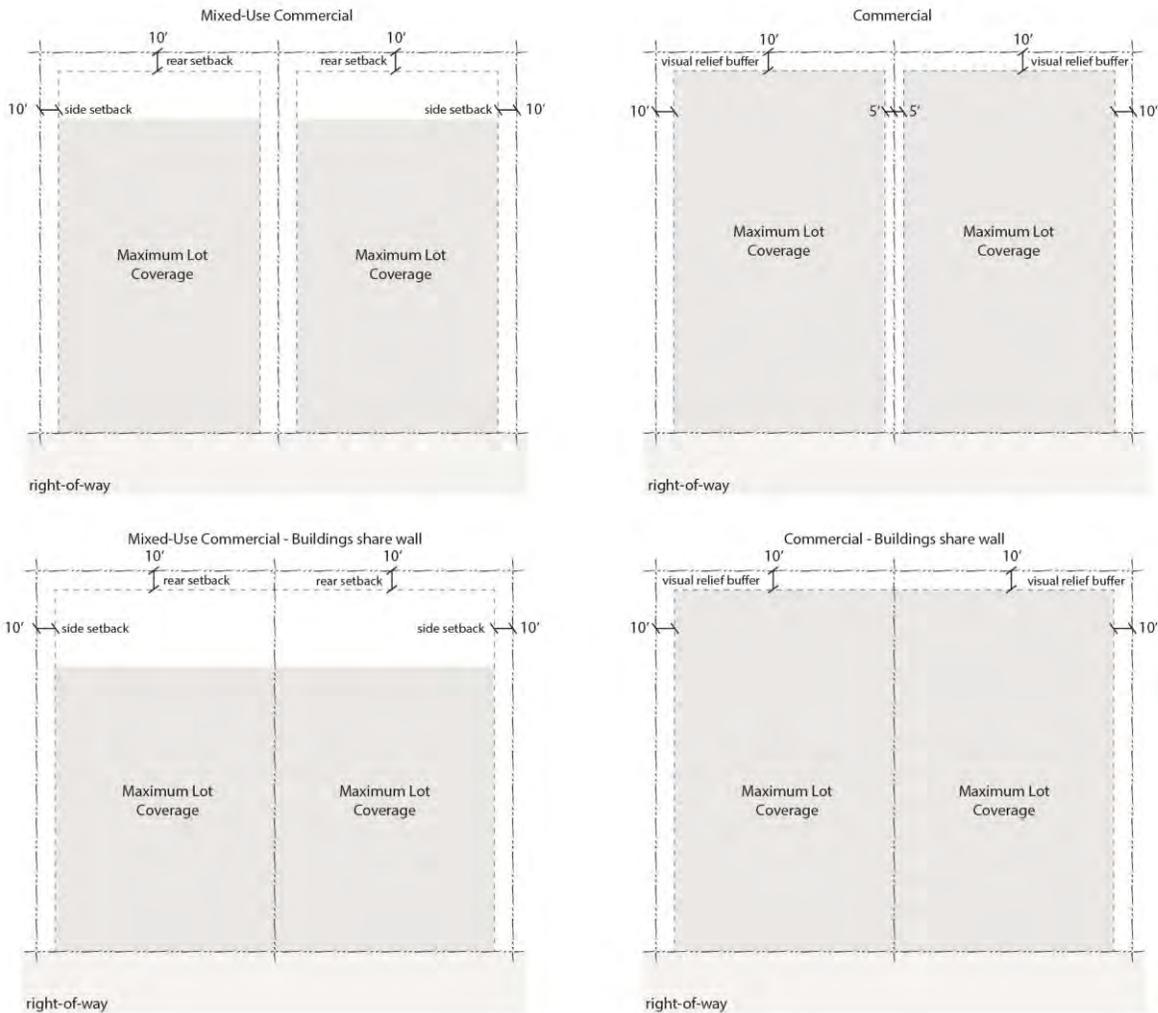
- a. Buildings shall provide a primary entrance facing River Road. Primary entrances shall be clearly identifiable as the primary entrance to a building and oriented to the sidewalk and street.
- b. A minimum of one Foothills Trail-oriented public entrance (secondary entrance), accessible during normal business hours, is required for all buildings located less than 50 feet from the Foothills Trail.
- c. If a secondary, or Foothills Trail-oriented public entrance is not provided, a public walkway accessing the primary street-oriented entrance from the Foothills Trail must be provided. If located along a shared lot line, public walkways may be consolidated between adjacent lots.
- d. Public entrances shall be made visible and prominent using architectural elements, such as canopies, fixed seating, large doors, porches, or protruding or recessed entrances. Primary public entrances shall be enhanced by no less than three of the following:
  - i. Provide at least 200 square feet of landscaping.
  - ii. Provide pedestrian facilities, such as benches, special paving, or bicycle racks.
  - iii. Provide a trellis, arbor or other building element that incorporates landscaping.
  - iv. Provide adjacent window displays.

- v. Architectural details integrated into the building structure and design.
- vi. Provide artwork or special pedestrian scaled signs.
- vii. Other methods approved by the director as meeting the intent.

**IV.D.4 Parking And Vehicle Access:**

- a. Parking shall primarily be provided on River Road. When the required minimum number of off-street parking spaces as dictated by BMC §19.28.040 cannot be accommodated on River Road, off-street parking shall be located in City designated parking areas and designed to meet the following standards.
  - i. Vehicle parking shall not be located between the Foothills Trail and the back of the building.
  - ii. Parking lot entrances (curb cuts) should not interrupt the level grade of the sidewalk.
  - iii. Parking lot entrances and exits must be as narrow as fire code will permit.

*Example: Building Setbacks*



**5. Screening Dumpsters, Utilities and Service Areas:**

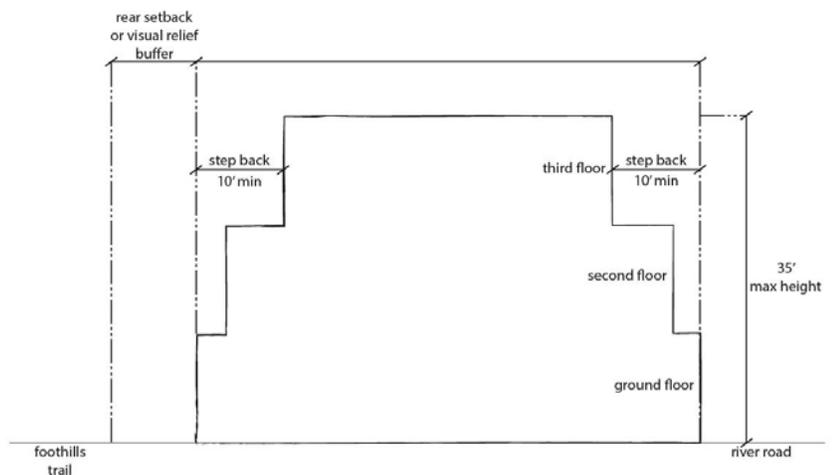
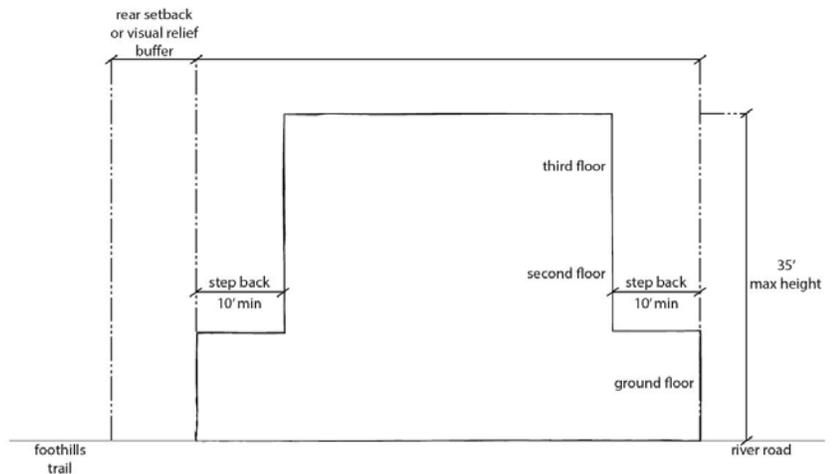
- a. Service areas (loading docks, trash dumpsters, compactors, and mechanical equipment) shall located to avoid negative visual, olfactory, auditory (noise), or physical impact on the street or trail environment.

- b. Service areas should be located to one side of the buildings, and should be visually diminished using screening by landscape or architectural features in a manner that is compatible with the building and site design.
- c. All rooftop equipment (mechanicals, HVAC) should be screened from public view at street level.



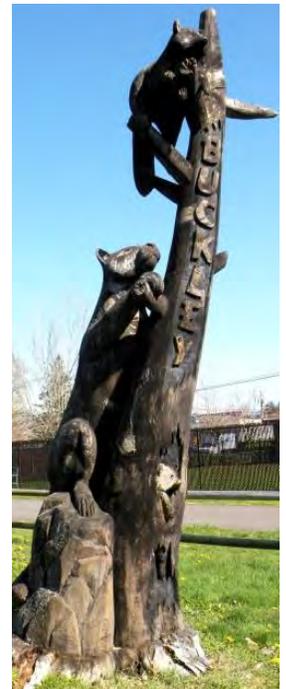
#### IV.E Architectural Design

The Rainier Gateway Subarea is considered "Buckley's front yard." Similar to a front yard meeting the street, a proposed building fits into its context by relating to the public right-of-way. The character of the public right-of-way is determined by the width of the right-of-way, landscaping and street trees, setbacks, height, width, orientation, and architectural style of the buildings. The following design guidelines deal with general design concepts as well as specific and prescriptive design elements intended to guide the design of new construction in the Rainier Gateway Subarea.



## 1. Massing And Articulation:

- a. Building Height. Buildings shall be a minimum of two stories and a maximum of three stories or a total of 35 feet in height. The ground floor shall have a minimum height of 12 feet as measured from finished floor to ceiling.
- a. Building Articulation. Buildings shall include articulation along the facades facing and visible from River Road or the Foothills Trail. All new development shall employ at least two of the following elements. If the proposed building is more than 100 feet wide as measured along any façade facing the street or trail, then the building shall use at least three of the following elements:
  - i. Changes in materials.
  - ii. Individual bays or protruding storefronts.
  - iii. Balconies or decks on upper floors.
  - iv. Separated, distinct roof forms.
  - v. Extrusion of corners.
  - vi. Distinct window patterns.
  - vii. Changes in colors.
  - viii. Other features approved by the director as meeting the intent.
- b. Horizontal Building Modulation. The maximum façade width along the facades facing and visible from River Road or the Foothills Trail, as measured horizontally along the building exterior, without building modulation shall be 25 feet.
  - i. The minimum width of modulation shall be ten feet.
  - ii. The minimum depth of modulation shall be two feet.
  - iii. Building frontage at the ground level may not modulate in depth more than six feet. Roof decks, balconies, or bay windows may be used as all or part of the building modulation and they may exceed this requirement.
- c. Roof Line Modulation. Modulate the roofline of all facades visible from a public right-of-way, the Foothills Trail, parking areas, or open spaces. Roof forms must include one or more of the following:
  - i. Provide a gable, hipped or shed roof with a slope of at least six feet vertical to 12 feet horizontal. The individual segments of the roofline must not extend more than 25 feet in width, measured horizontally.
  - ii. The width of a continuous flat roofline shall extend no more than 25 feet without modulation. Modulation shall consist of either:
    1. A change in elevation of the visible roof line of at least four feet;
    2. A sloped or gabled roof line segment of at least ten feet in width and no less than three feet vertical in 12 feet horizontal; or
    3. A combination of both 1) and 2).
  - iii. Coordinate modulation of rooflines on multi-tenant buildings to highlight key building entries and/or change in building tenants or uses.
  - iv. Use roof modulation in mixed use or multifamily buildings that employ:
    1. A gable, gambrel or hipped roof;
    2. Broken or articulated roof line;



3. Prominent cornice or fascia that accentuates the top of the building; or
4. Other roof element that accentuates the buildings concept and helps it fit in with neighboring structures with prominent roofs.
- v. Alternative designs subject to approval by the director, if they demonstrate that the proposed design meets the intent of the standards.

## 2. Upper Level Step Backs:

- a. Building façades shall be stepped back above the first story on both the street and Foothills Trail frontages for residential uses. The intent of this section is to provide outdoor living space for apartments and homes that do not have direct access to ground floor outdoor space. This is not intended to produce a rigid uniform step back, and as such the following alternatives are acceptable:
  - i. The top floor steps back at least ten feet.
  - ii. All floors above the ground floor step back at least ten feet.
  - iii. Progressive step backs on the second and top floor to a total of at least ten feet (e.g. second floor seven feet, top floor three feet).
- b. Facades of floors that are stepped back shall be distinguished by a change in elements such as window design, railings, trellises, details, materials and/or color so that the result is a rich and ordered combination of features that face the street.
- c. Balconies may extend into the step back areas. Seating and dining areas may be provided on upper story step back areas oriented towards the Foothills Trail.
- d. Alternatives to this design guideline may be proposed, so long as the effect is that the upper floor(s) appear to recede from view.

## 3. Streetscape:

- a. Blank Walls. Blank walls are not allowed adjacent to, or within 25 feet of a public right-of-way, the Foothills Trail, parking areas or open spaces. At least 60 percent of the wall area between two and 12 feet above grade must be pedestrian friendly. Pedestrian friendly facades shall have one or more of the following characteristics:
  - i. Transparent Windows. Transparent window area or display windows, which provide visibility into building interiors. The following transparency standards apply:
    1. Glass must be clear or lightly tinted in windows, doors, and displays. Reflective, opaque, or painted glass is prohibited.
    2. Doors and entry windows must be transparent to meet this requirement. If they are not, they shall be considered blank walls.
  - ii. Art or Architectural Treatment. Sculpture, mosaic, mural or similar relief artwork that provide visual



interest. Structural architectural elements may be acceptable if the design meets the intent of this section.

- iii. Vertical Trellis or Planting Bed. A permanent vertical trellis in front of the wall with climbing plants or planting bed with plant materials designed to obscure 60 percent of the wall's surface within three years.
- iv. Display Windows. Display windows may be used to meet this requirement.
- b. Transparency. Commercial mixed-use buildings shall include windows with clear glass on at least 30 percent of the area between two and 12 feet above grade for all ground floor building facades that are visible from a public right-of-way or the Foothills Trail.
- c. Weather Protection. All businesses shall provide pedestrian weather protection at least four feet in depth along at least 75 percent of the length of building facades that are adjacent to River Road, and at least 50 percent of the length of building facades that are adjacent to the Foothills Trail or provide pedestrian connections to the trail. The weather protection may be in the form of awnings, canopies, building overhangs or architectural details to create covered outdoor pedestrian space.
  - i. Pedestrian weather protection shall be provided at public entries.
  - ii. Canopies or awnings shall be placed between eight feet and 12 feet above the sidewalk and have a minimum depth of six feet. Color and material of pedestrian coverings shall complement building colors and be approved by the City. All lettering and graphics on pedestrian coverings shall conform to BMC §19.30.

#### **4. Architectural Elements:**

- a. Doors. Doors shall be simple in shape. Painted wood doors and wood framing are preferred over aluminum doors. Wood doors may incorporate windows to meet transparency requirements.
- b. Windows. Windows shall be fixed, casement, awning, arched, or double hung, with a square or vertical orientation. Sliding windows are not acceptable. Wood or metal sash materials are acceptable. Windows shall cover a minimum of 30 percent of the Foothills Trail fronting and street fronting façade area of ground floor commercial or mixed-use buildings. Windows shall begin at least one foot above grade.
- c. Newer, energy efficient window types may be considered when constructed in a style that compliments the intent of these design guidelines.
- d. Roofs. Roof forms shall be in keeping with existing building forms along the Rainier Gateway Subarea and Main Street. Flat roofs with dominate cornices are the predominant roof form on Main Street. Buildings with sloping roofs shall have a minimum pitch of six feet vertical to 12 feet horizontal. Roof forms shall incorporate simple gables or parapets, and sheds with generous overhangs, particularly over primary entrances.

#### **5. Exterior Materials:**

- a. Buildings shall be constructed from materials that are similar in texture and finish to those found along Buckley's historic Main Street. The use of natural materials that reflect the Northwest setting such as brick, local woods, and stone are encouraged. The choice of materials shall also reflect the building's intended use.
- b. The following materials shall be used accordingly:



- i. Wood: Whenever visible from the exterior, wood truss and heavy timber structural systems are recommended. Horizontal sidings such as clapboard and tongue-in-groove, vertical siding such as board and batten, and other horizontal sidings such as smaller wood shingles is required. Larger, more rustic styles of shakes may be appropriate.
- ii. Brick: Full size brick is preferable to thin veneer brick. When used, brick veneers shall be mortared to give the appearance of full-depth brick and detailed with wrap-around corner and bullnose pieces.
- iii. Stone: Complimentary stone or stone veneers shall be used along with wood finishes. When required by building code or for functional reasons, concrete and concrete masonry materials may be employed, provided it is designed to emulate traditional stone construction.
- iv. Metal siding: Historically appropriate metal siding may be used either as an accent material or to achieve a special architectural character. Metal siding may also be approved where it mimics the appearance of another approved building material.
- c. To keep in character with the surrounding area, high gloss finishes are not permitted. Flat or low gloss finishes shall be used.
- d. If no paint is used, stain and/or oil shall be applied to protect and preserve the natural material from the elements.
- e. Wood shingles or metal roofing shall be employed on all new construction.

6. Exterior Colors:

- a. Exterior paint colors shall mimic the rural and natural features in and around Buckley. They should create a harmonious effect and be selected with care.
- b. The predominant color shall be muted; natural/earth tones and simple painting schemes are acceptable.
- c. Color schemes employing many colors, particularly bright colors, or contrasting colors shall be avoided. The color of brick or other natural building materials should dictate



- the color family choice. Natural, unpainted wood with a clear sealer or wood painted grey, dull red or green are examples of appropriate color palates.
- d. Colors shall be selected to highlight building form and highlight major features.
  - e. Warm colors at building entrances are recommended.
  - f. The use of accent colors on features like doors, signs, or windows is encouraged, but accent colors should not detract from the overall appearance.
  - g. The number of accent colors used shall be limited to a maximum of two.

***Example: Buildings with Good Articulation and Modulation***



## V. SR 410 Design Guidelines

### V.A. Introduction

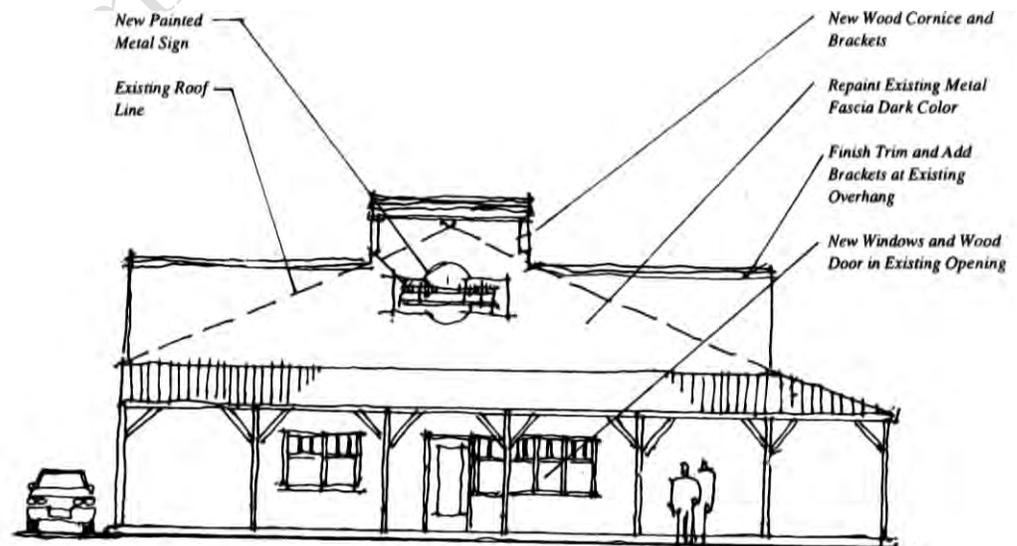
While the downtown review district represents much of Buckley's past, defining its current image and character, the land along Highway 410 will contain much of Buckley's future. As the city and region grow this open, level land will be increasingly converted from agricultural and low density development to commercial and industrial uses. How this change is allowed to take place will in large measure influence the character and quality of life for the citizens of Buckley into the next century.

### V.B. Purpose

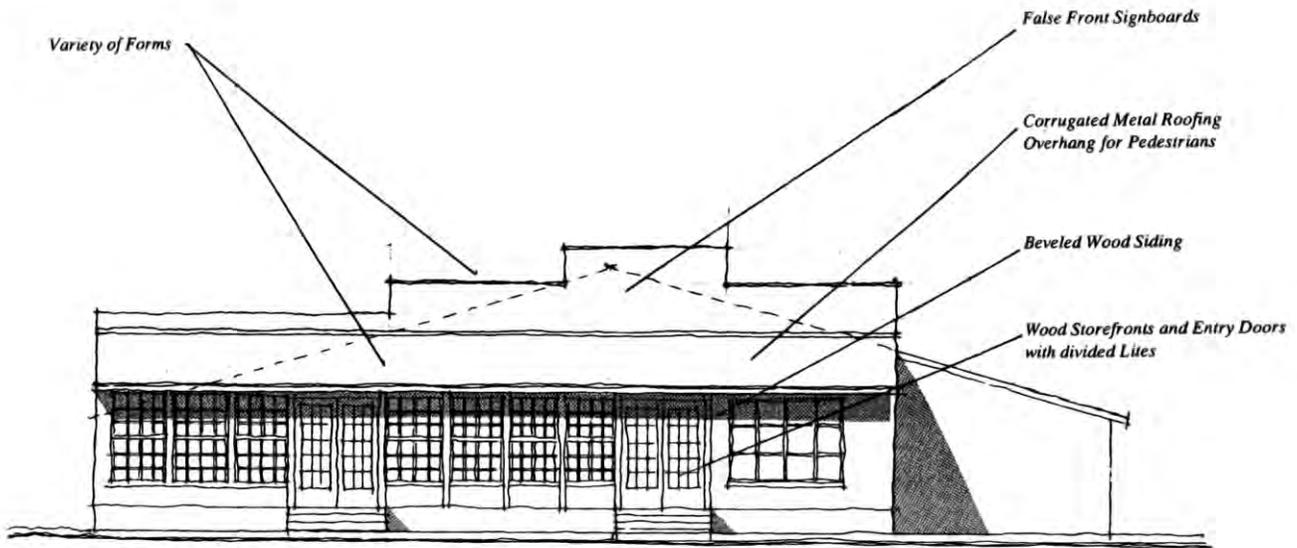
The Highway 410 corridor provides visitors and passers-by with their first and sometimes only impression of the city. It should be considered Buckley's front yard. A favorable impression along the highway increases the potential for visitors to turn into Main Street for further exploration of the community.

As previously stated, the *goal for the downtown design district* is restoration and rehabilitation of the current building stock to its original appearance. The introduction of unifying elements such as signs, street furniture and awnings help tie together the individual properties and create a sense of place. This reinvestment will allow the downtown area to continue as a commercial center for Buckley.

In contrast, new development on the highway will be large in scale flanked by large parking areas, approached by care, and viewed at relatively moderate speeds. A variety of uses must share the area; large retailers, manufacturers and higher density residential developments, Unlike the downtown area, new buildings on the highway will function and be seen as individuals, not connected by shared walls and facades. These building must take a different form that suits their purpose without destroying the character and individuality of Buckley.



*Example: Mr. Auto Renovation*



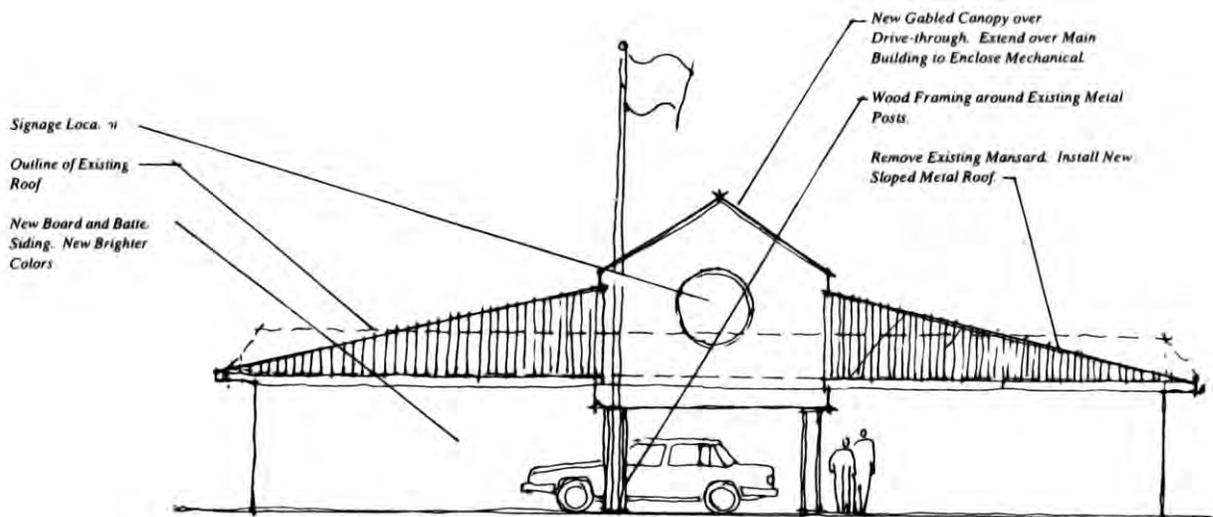
*Example: Single Story Retail*

The Buckley plateau and surrounding hills contain many fine examples of architecture built by settlers and those who came after them. These barns and lumber mills look at home in the landscape because their forms are simple and straightforward, accomplishing their purposes without gimmicks or unnecessary ornament.

Construction is principally of wood, stone and metal, with long horizontal lines and sloping roofs. These large storage and work buildings with long roof spans and tall ceilings make appropriate and natural models for new construction along Highway 410.

**V.C. Regulations and Applicability**

The following guidelines for development in the Highway 410 Review District are based on creative and sensitive reintroduction of these traditional building materials and forms to new



*Example: Eagles Lodge Renovation*

commercial and industrial uses. The guidelines also require unifying elements for the overall development of properties along Highway 410 in designated review areas.

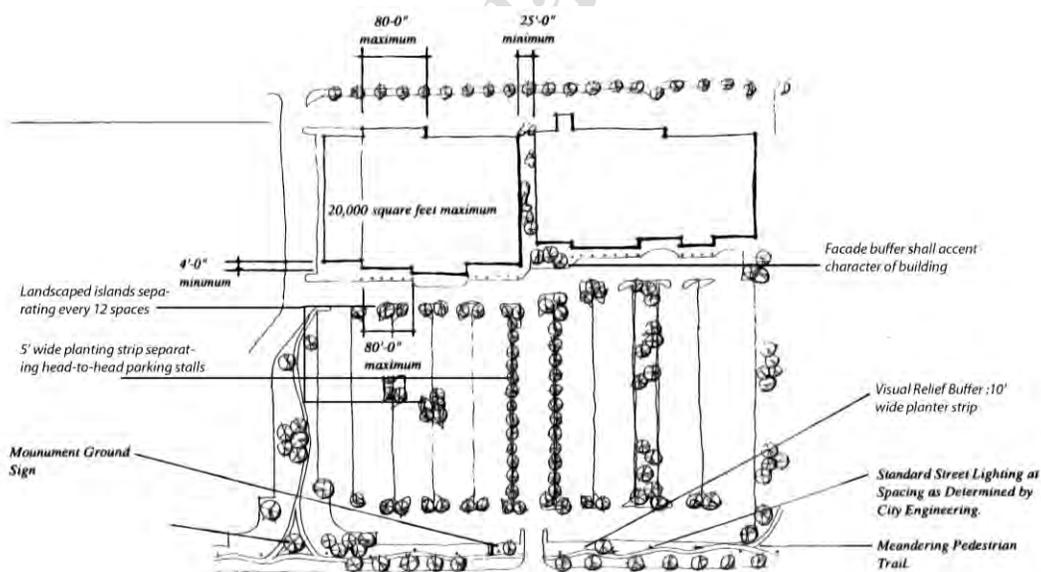
New developments and renovation of existing buildings and properties serving current and new uses are required to comply with these guidelines as interpreted by the DRC as a part of the permit process.

## V.D. Architectural Design.

Building and new development along Highway 410 should be designed to reflect and reinforce the rural character of the area. Buildings should be reminiscent in form, material and detail of the traditional types of building in rural Pierce County, and particularly in Buckley Plateau. Traditional structures were primarily built here for the agricultural and timber industries, and they are straight-forward and functional in appearance.

### 1. Site Development.

Site design can depend on uses, but the design displayed to visitors and people driving through the city from one destination to another should create the feel of a small town with a character described in the city's comprehensive plan. This area is decidedly auto-oriented, but the small town character is not. Development in this district should combine the two needs by placing a commercial-looking establishment with no more than two parking lanes between the highway and the buildings. Industrial uses, if allowed by zoning, such as outdoor storage, can be placed behind the building.

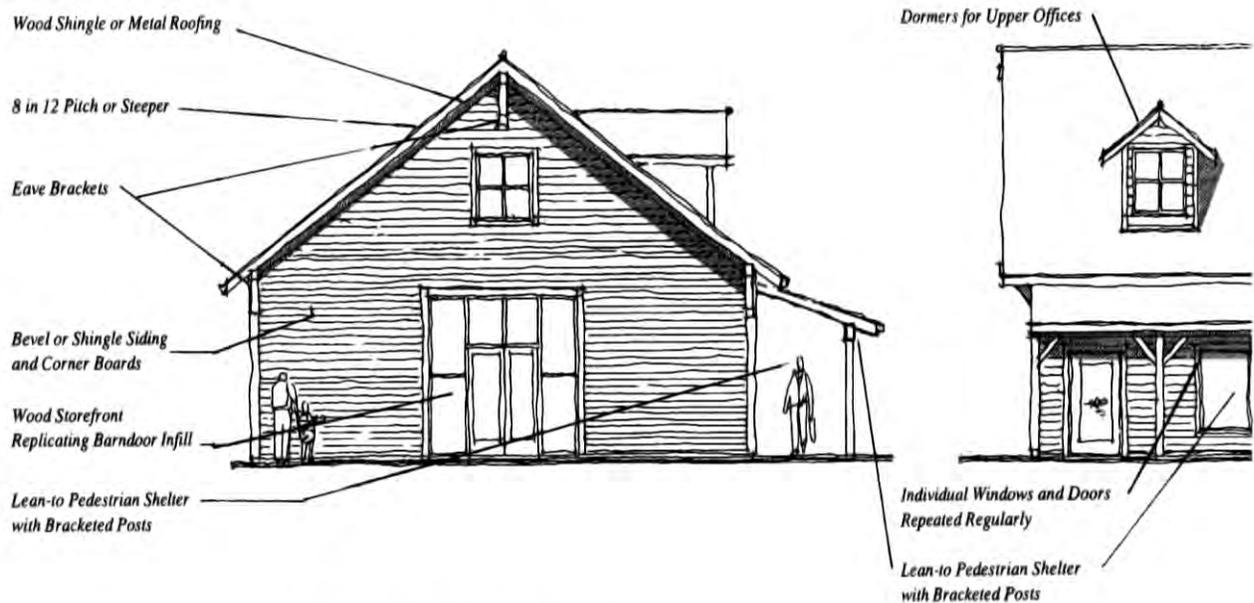


Example: Commercial Site Development

### 2. Scale and Form

Projects shall relate to the scale and form of traditional agricultural and mill buildings. Traditional buildings often were added onto over the years. Farms and mills usually consisted of a cluster of similar buildings. New construction shall utilize these devices wherever possible to reduce the scale and apparent mass of larger projects.

- a. Long, uninterrupted buildings and developments are not acceptable. Modulation and/or variation of the building façade is required by shifting the elevation no less than four (4) feet every eighty (80) feet of building length.
- b. Individual buildings or developments shall not exceed 30,000 gross square feet in area.
- c. Individual buildings within overall developments shall be separated by a landscape buffer unless otherwise approved by the DRC. Covered walkways may connect across buffer spaces.



*Example: Two Story Retail Office*

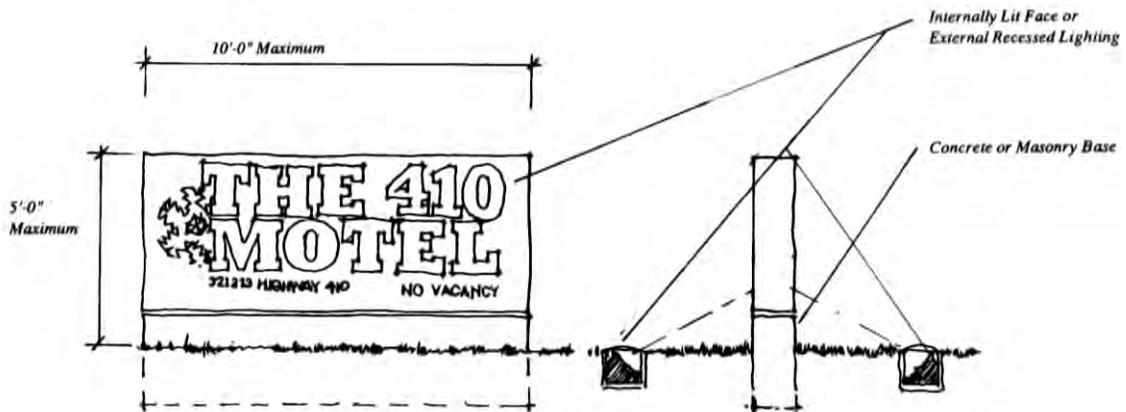
- d. The architectural design of new projects shall take into consideration and respond to the scale and character of the rural landscape.
- e. Overall architectural design shall be based on traditional agricultural and mill buildings.

### 3. Architecture and Structure

- a. Where visible from the exterior, wood truss and heavy-timber structural systems are recommended along with complimentary stone veneer(s).
- b. Buildings shall be no more than two stories and shall have sloping roofs with a minimum pitch of *4 in 12* unless otherwise allowed by the DRC.
- c. Roof forms shall incorporate simple gables and sheds with generous overhangs.
- d. Siding: Utilize horizontal beveled wood, wood shingle or vertical wood board and batten siding along with complimentary stone veneer(s) whenever possible. Where required by building code or for functional reasons, concrete and concrete masonry materials may be employed with approval of the DRC. In such cases concrete and concrete masonry shall be detailed to emulate traditional stone construction.



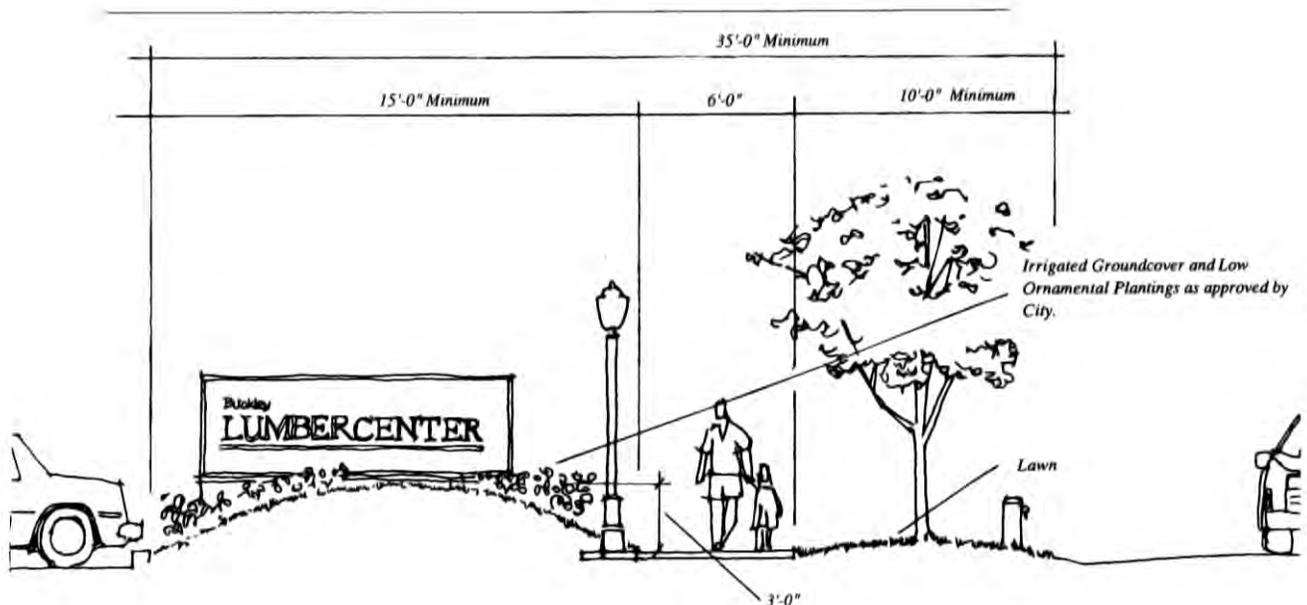
- ii. Colors should be selected to emphasize building form and highlight major features. The use of brightly colored or glossy building materials such as metal or tile siding are not acceptable.



b. Signs, Monument Signs.

Monument Ground Signs identifying a building or complex. These are large, direct signs that can be identified and understood at highway speeds from some distance away. The goal of this type of sign is to communicate a simple message clearly and in a manner that does not detract from the landscaped development of the highway.

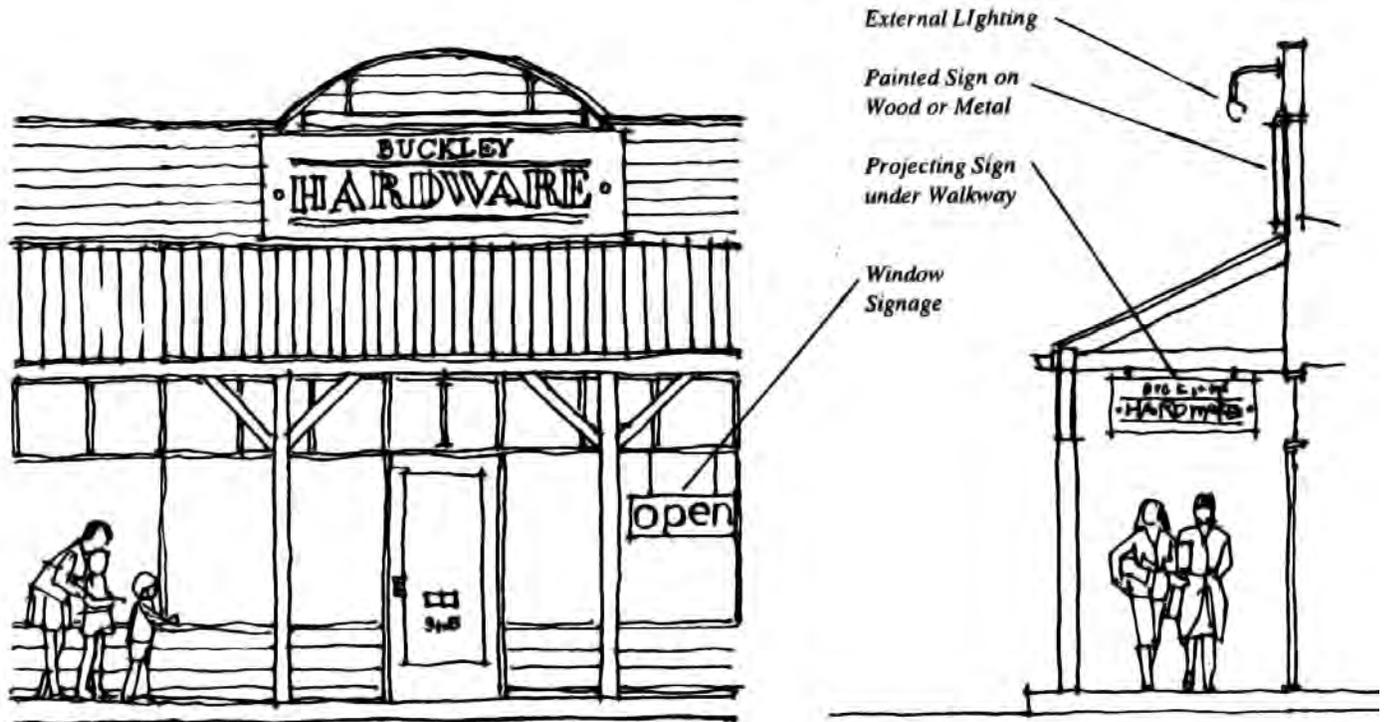
- i. Monument Ground Signs: Signs identifying individual business buildings or multiple



tenant complexes shall be coordinated with the City of Buckley's streetscapes concepts for the Highway 410 frontage.

- ii. Sign body construction shall of concrete, wood or metal set on a concrete stone, or masonry foundation or base.
  - iii. All monument signs shall fall within the required landscape buffer along Highway 410.
- c. Signs, Building Mounted (Façade).

- i. Signs attached to a building that identifies the tenants and activities at a pedestrian oriented scale. These signs serve a similar purpose to the signs along downtown and shall be similar in design.
- ii. Wall signs may be flush with or projecting from the building face.
- iii. Projecting signs may not extend more than four (4) feet from the building.



# CountyView Web Map



## Map Legend

Highlighted Tax Parcels

### Tax Parcels

- Base Parcel
- Condominium
- Other

### Roads

- Interstate
- Limited Access State Routes
- Other State Routes
- Ramps
- Major Arterial
- Collector
- Local Access

### Zoning - Buckley

- CC
- GC
- HC
- HDR
- LI
- NMU
- P
- R-20,000
- R-6,000
- R-8,000
- S

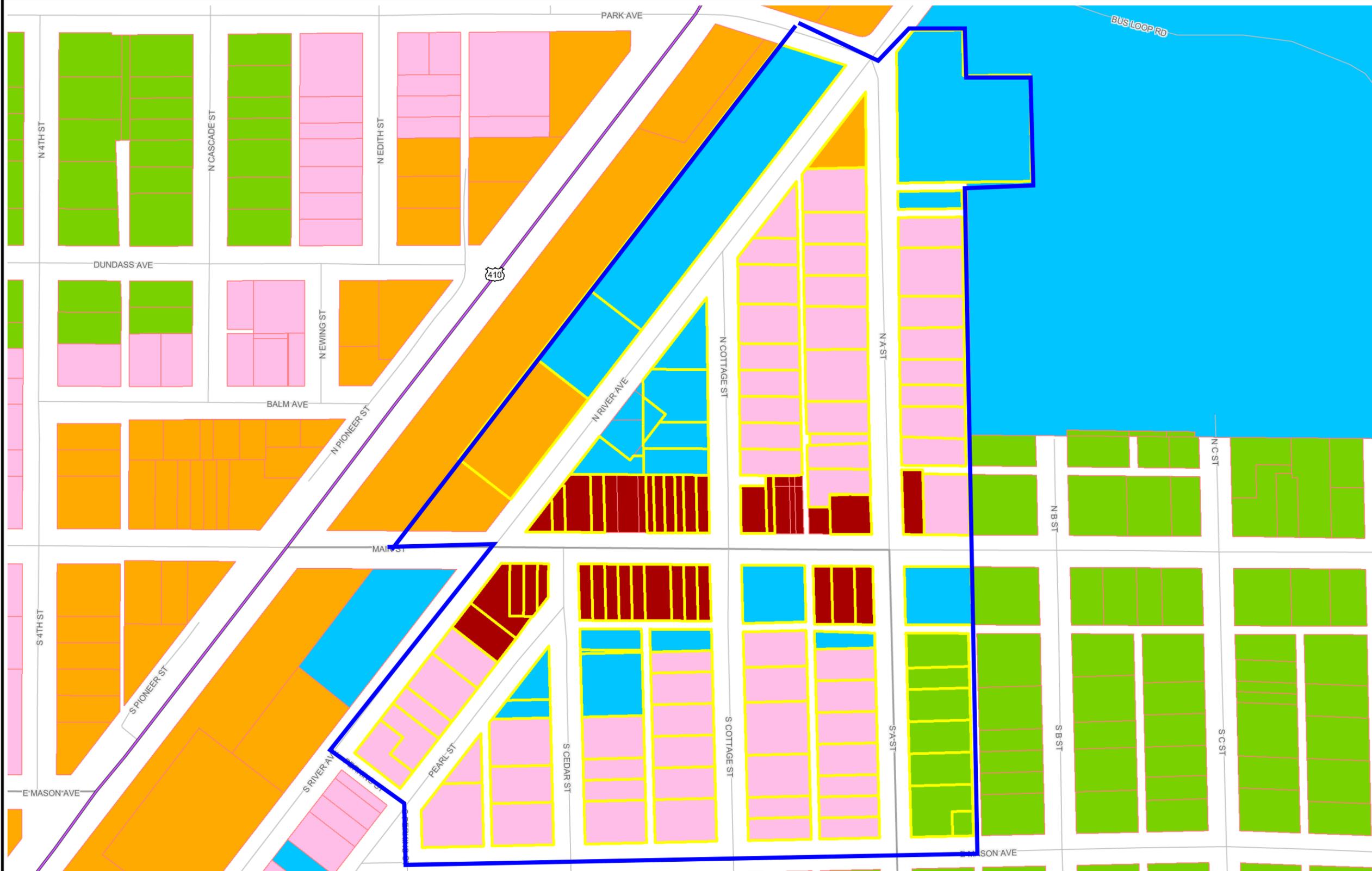
Recommended boundary for Downtown Design District

Scale 1:2,268

0 95 190 ft.

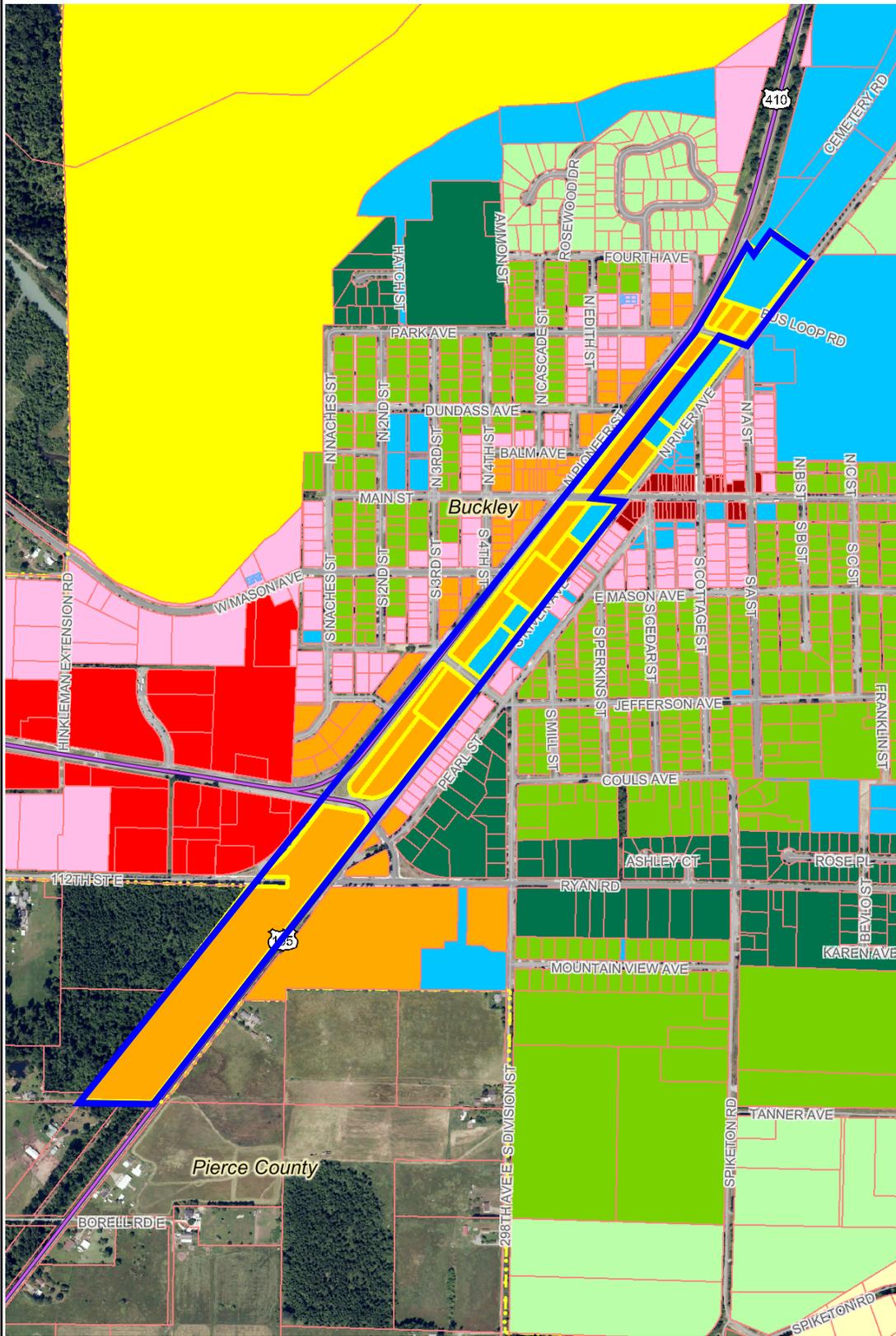


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# design: gateway district zoning



## Map Legend

Highlighted Tax Parcels

### Tax Parcels

- Base Parcel
- Condominium
- Other

### Zoning - Buckley

- CC
- GC
- HC
- HDR
- LI
- NMU
- P
- R-20,000
- R-6,000
- R-8,000
- S

### Comprehensive Plan - Buckley

- C/MU
- I/GC
- UHD
- ULD
- Right of Way

### Pierce County Basemap

#### Roads

- Interstate
- Limited Access State Routes
- Other State Routes
- Ramps
- Major Arterial
- Collector
- Local Access

#### County - 2014 - Ortho

0 950 ft.



Scale 1:11,531

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# CountyView Web Map



## Map Legend

Highlighted Tax Parcels

### Tax Parcels

- Base Parcel
- Condominium
- Other

### Zoning - Buckley

- CC
- GC
- HC
- HDR
- LI
- NMU
- P
- R-20,000
- R-6,000
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### Pierce County Basemap

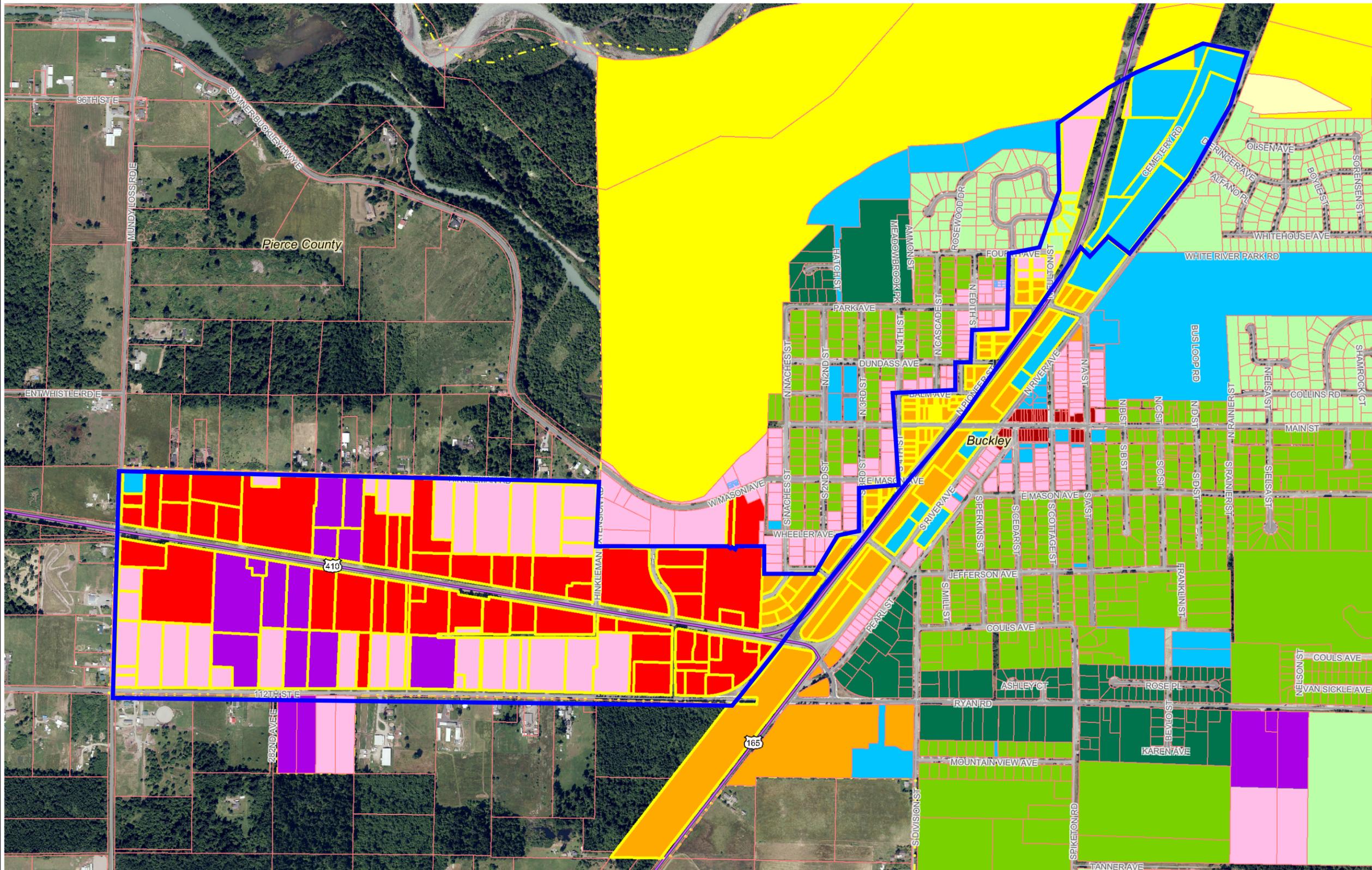
#### Easements - Lines

- City of Tacoma Easement
- Gas Easement
- Monument Line
- Private Road
- Public ROW
- Railroad
- Stormwater Easement
- Transmission Easement
- Water Easement

#### Roads

Scale 1:10,242

0 425 850 ft.



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