

City of

BUCKLEY

Guidelines for Redevelopment

ACKNOWLEDGEMENTS

City of Buckley, Washington
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1. Introduction and Purpose

The citizens of Buckley have come to understand that the quality of life in their city is one of their most valuable assets. The sense of community, availability of recreational opportunities, access to open space and close proximity to urban centers make Buckley an ideal place in which to live and raise a family. It is recognized that the built environment has a great impact on this quality of life. As business and residential areas grow closer to the city limits it will be increasingly difficult to maintain a unique identity as an independent municipality. In order to do so Buckley must recognize and preserve its history.

To make the correct decisions for the future of the City of Buckley it is important to understand the past. Buckley is one of the oldest communities in Pierce County and has long been a crossroads of activity for railroad, lumber, agricultural and mining interests. As each of these industries grew, a portion of the built environment was shaped, and traces of each are still present in the buildings and streets of the city.

The plateau on which Buckley is sited was an early trading route through the Washington Territory, with the Naches Pass Trail and Military Road passing nearby. Later, in the 1880's, the Northern Pacific Railroad established a siding at White River as a part of its transcontinental rail link to the East coast.

To recognize the influence of the railroad, and in hopes of securing its further interests, the town was named Buckley for a division superintendent of the railroad in 1888. While the anticipated railroad boom never fully materialized, the town was physically shaped by the railroad. To this day the buildings along River Street show evidence of their use as railroad warehouses, and the angle of River Street parallels the tracks, creating interesting intersections at an angle to the dominant street grid.

Through the late nineteenth and early twentieth centuries Buckley grew slowly, remaining somewhat isolated from communities in the valley below. It served as a

local center of trade for farmers and loggers.

The business district, gutted by fire in 1892 and again in 1925, began to replace its simple wood framed storefronts with brick and cast iron buildings, and took on the appearance of an established city. Many of these buildings are good examples of the commercial architecture of the day.

As the influence of the automobile grew after World War II and the markets and services in Tacoma and other towns became more accessible, Buckley's downtown came under increasing economic pressure. As in many places, the suburban supermarkets built in the fifties and sixties brought difficult times to this center of town, traditionally the hub of community life.

In the seventies and much of the eighties Buckley's relatively remote location and surrounding farms helped to keep it insulated from the suburban sprawl that has blurred the identities of other small towns in western Washington. But as the population

of Pierce County increases and farms are sold for other uses this insulation is beginning to disappear, making changes in Buckley inevitable.

While the City of Buckley welcomes the opportunities this growth may make possible, it is important to maintain the character and quality of life that make the city a desirable place in which to live.

To this aim, the City has established an Architectural Review Board. This citizens' panel is charged with the review of proposals for restoration and new development in these key districts and in other areas as directed by the City Council. Proposed projects submitted to the Architectural Review Board must meet the development standards outlined in these guidelines.

The following Redevelopment Guidelines illustrate the architectural features that are important to maintaining the character of the business district and to establishing an appropriate identity for the town as it grows along Highway 410.

2. Redevelopment Approach

The character of Buckley's buildings is based on the city's development as a working community with ties to farming, lumber and the railroad. Its commercial buildings are well constructed and are interesting not because of elaborate ornament, but for their history, craftsmanship and proportion. Buildings outside of town have always been utilitarian structures that stress strong, simple, functional forms over decoration. It is not the intent of these guidelines to introduce a clever theme or arbitrary style to the city, but rather to promote the honest restoration of existing structures and the construction of new ones in a manner that is in keeping with the history and spirit of the community.

To coordinate future development the City of Buckley has, through ordinance, established an Architectural Review Board made up of citizen volunteers appointed by the City Council. Any and all exterior construction, reconstruction or remodelling of any structure, excluding single family dwellings, within the two Design Review Dis-

tricts established by the City Council must be reviewed and recommended for approval by the Architectural Review Board as a part of the building permit process.

Buckley's Design Review Districts are defined by City ordinance, and can be described as follows:

1. **Downtown Review District:**
Includes Main Street and extends from River Avenue to the east side of the alley between A and B Streets.. The District includes properties north through the White River School District property to River Avenue, and south to Mason.
2. **Highway 410 Review District:**
All property abutting the highway within the city limits in B1, B2, RB, IP and P zones, and property in zones adjacent to these zones.

Submittals to the Architectural Review Board for proposed new construction and remodelling projects within these zones must be made in the following format. Incomplete submittals may not be reviewed.

ARCHITECTURAL REVIEW BOARD SUBMITTAL FORMAT

A. General Requirements:

All drawings (each sheet) must contain the following information:

1. Project Name.
2. Project Address.
3. Date.
4. Name, address and phone number of firm responsible for drawings.

B. Required Information:

1. Site Plan showing the following:
 - a. North arrow.
 - b. Drawing scale: site development plans to be drawn at 1" = 20'-0" or 1" = 30'-0".
 - c. Existing buildings to be removed or retained.
 - d. Major landscape features.
 - e. Existing and proposed topographic contours at 2'-0" intervals.
 - f. Existing streets and roads on or bordering property, including curb lines and sidewalks.
 - g. Existing buildings and features within 100 feet of subject property lines.

- h. Exterior lighting locations and types including mounting heights.
- i. Exterior signage locations and types.

C. Statistical Information:

1. Area of proposed structure.
2. Lot coverage of structure.
3. Lot coverage by impervious surfaces.
4. Building height.
5. Number of parking spaces.
6. Area of landscaping.

D. Landscaping Plan:

1. Extent and location of all plant materials and other landscape features. Label individual plants or include a planting legend.
2. Proposed planting materials drawn to scale at mature sizes and indicating spacings. Indicate mature heights.
3. Species and size of existing planting materials to remain.
4. Location of water outlets or schematic irrigation system plan.

E. Exterior Building Elevations:

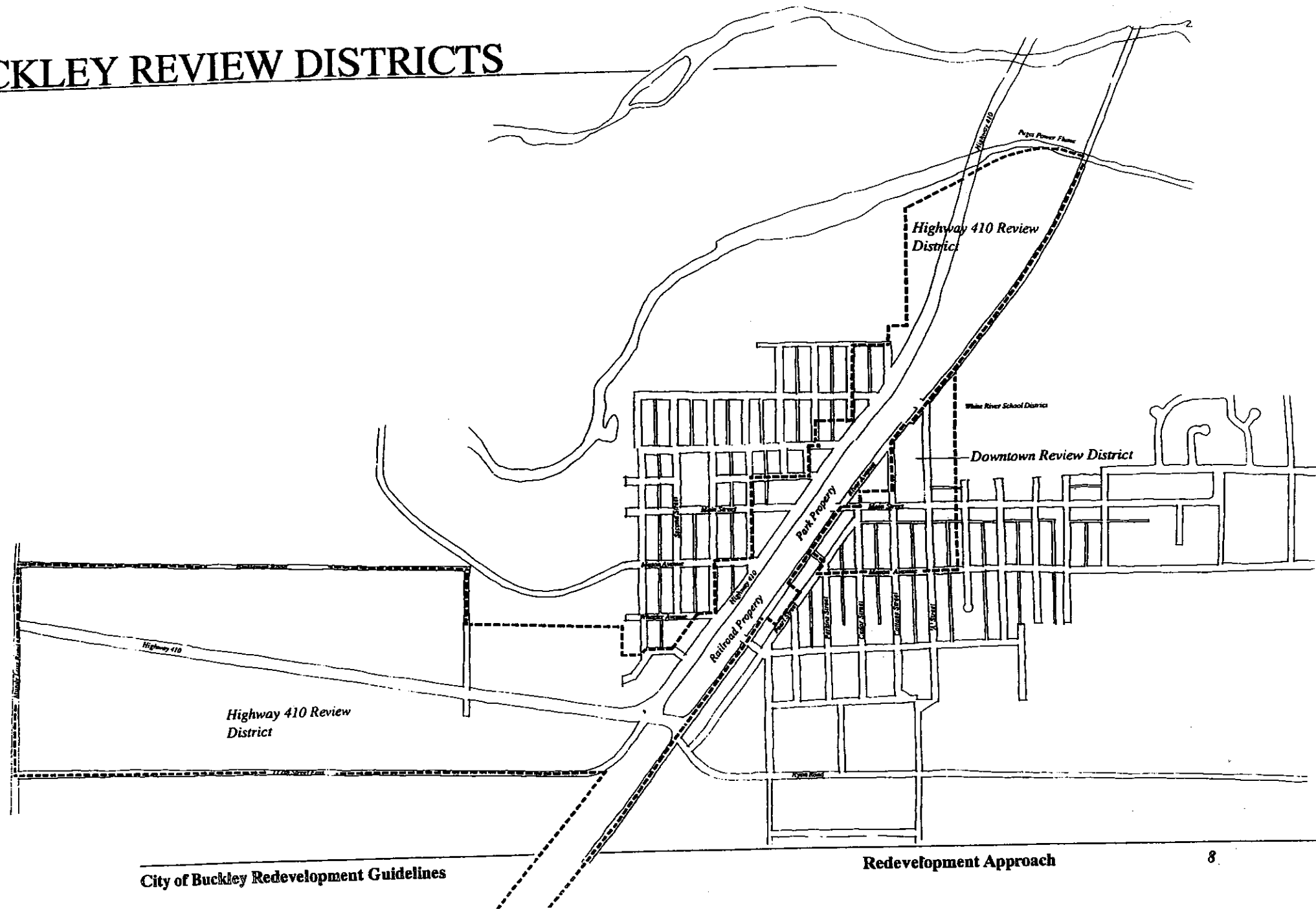
1. Architectural drawings at 1/8" = 1'-0" or 1/4" = 1'-0".
2. Indicate doors and windows, architectural details, materials and finishes.
3. Note existing and finished grades, base elevation and elevation of highest projection of building.
4. Provide color and exterior material samples.
5. Indicate signage to be located on building.

F. Architectural Details:

1. Details drawn to scale at appropriate sizes for all ornament, exposed structural systems and other details as required to describe proposal.

The Architectural Review Board may require additional drawings or submittals for specific projects. Additional submittals are required for building and fire department approvals.

CITY OF BUCKLEY REVIEW DISTRICTS



3. Downtown Review District

3.1 INTRODUCTION AND SCOPE:

The downtown review district contains a mixture of commercial, public and residential properties. Its buildings include the large masonry White River School District buildings, the wood frame warehouses of River Avenue, well maintained wood frame homes and a well defined commercial core on Main Street.

These redevelopment guidelines apply only to commercial and public structures. Single family residences are not subject to review unless they are to be converted to commercial use.

In the restoration of existing buildings downtown or construction of new structures, the elements that are most important to maintaining the architectural character of buildings on Main Street, River Avenue and the adjacent residential neighborhoods as defined by these guidelines or required by the Architectural Review Board shall be maintained or restored.

3.2 MAIN STREET BUILDINGS:

The buildings of Main Street are of several types differing in age, style and importance to the City's appearance. The buildings that make up Main Street's core have no setbacks from the sidewalk and share party walls at their side property lines. This is important to creating a feeling of enclosure and identity on the street.

3.2.1 Corner Buildings:

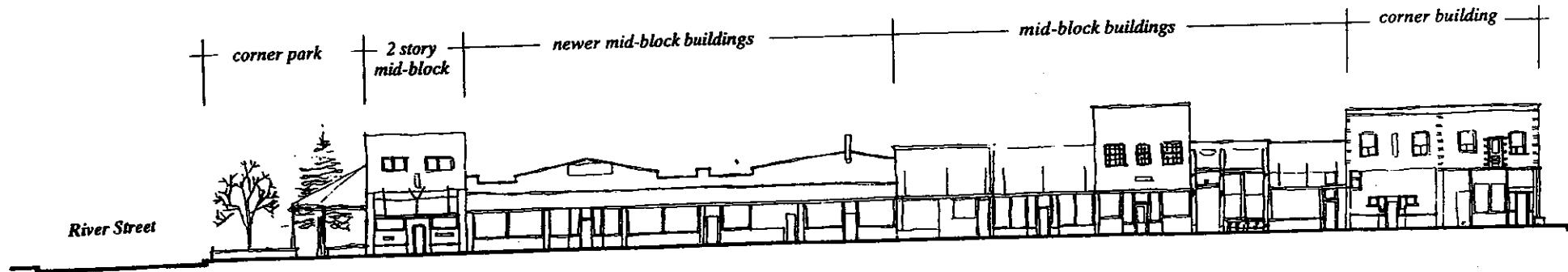
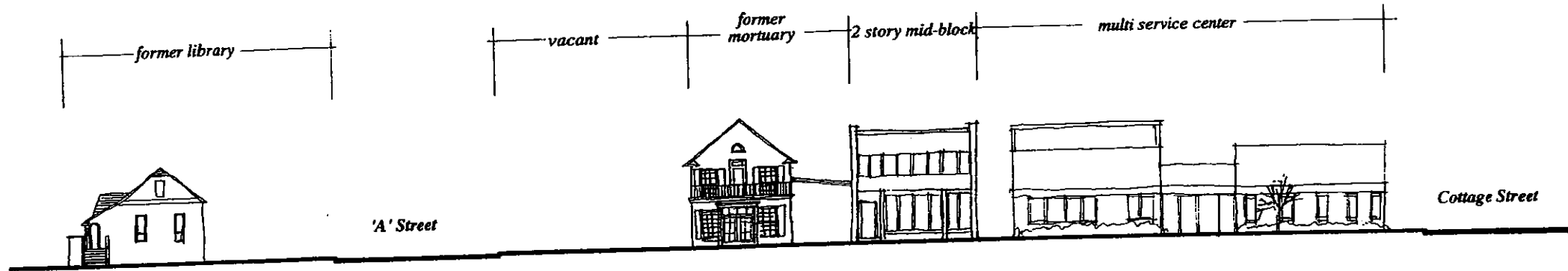
Some of the oldest remaining downtown structures are at the ends of each block. Typically they are unreinforced stone and brick, two stories tall with commercial tenants at street level and office or residential occupancies upstairs. These buildings add a great deal of character to Buckley, providing visual accents to define each major intersection. This oldest group of remaining buildings on Main Street dates from the early 1890's to about 1920, and contains many of the most distinguished structures in town. Buildings from this period reflect many elements characteristic of turn-of-

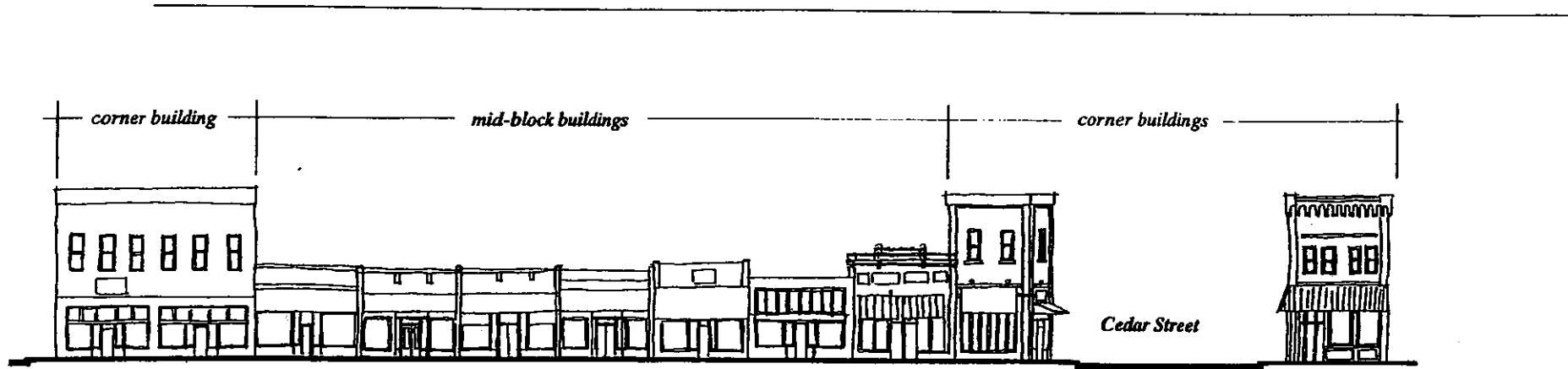
the-century architecture: traditional stone and masonry work, cast iron storefronts and stamped metal cornices were new innovations in their time.

Most of the two story buildings in Buckley were built with elaborate metal cornices and other ornament that adds an appropriate level of finish to their tops. Earthquakes and the effects of weather have removed most of these details, but evidence of their overall architectural effect is visible in historic photos of Main Street and in commercial districts of similar age in other cities.

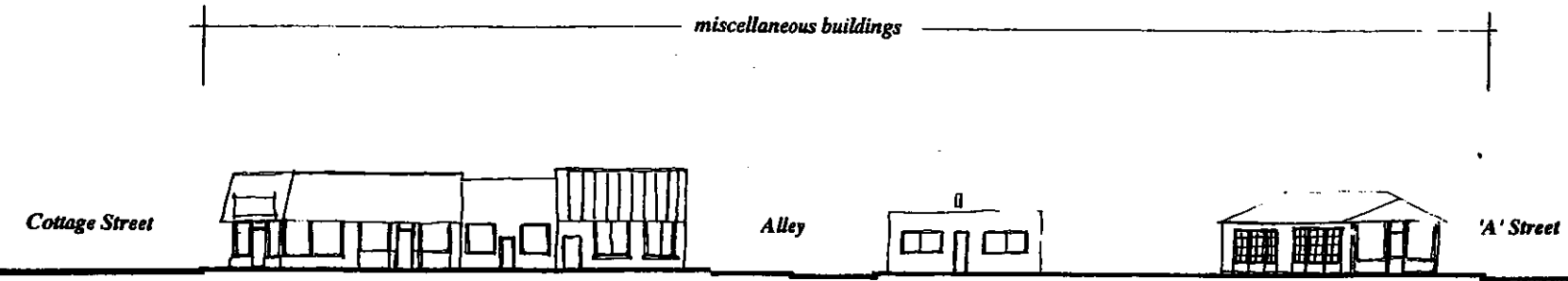
In typical buildings of this period, cast iron was often employed to imitate stone work and to form structural elements such as lintels or columns. Many of these elements remain but have been covered or disguised by non-conforming materials. Other cast iron elements have often been removed.

MAIN STREET





LOOKING SOUTH ON MAIN STREET



LOOKING NORTH ON MAIN STREET

The corner buildings include the following features:

- a. More than one story in height, usually with commercial spaces on street level and offices or residential space above.
- b. Traditional brick and stone construction, often with elaborate detailing.
- c. Flat roofs with dominant cornices.
- d. cast iron storefronts or wood storefronts.
- e. Wood sash double-hung windows.

These buildings are particularly important in providing definition to the corners and the ends of blocks. It is their dominance at intersections that gives Buckley much of its character.

3.2.2 Mid-Block Buildings:

Between these larger structures are two groups of one-story commercial buildings, the groups being identified by age and type of construction. They are all similar in height, scale and detailing, and serve to tie the blocks together.

Many of these structures are as old as the corner buildings, but are more modest in scale and detailing. While not the first structures you notice on a walk down Main Street, they provide much of the detail and pedestrian scale interest that can give Buckley's shops their individuality. The mid-block buildings share these elements:

- a. Typically twenty-five feet wide, brick construction.
- b. Usually a wood framed cornice on brackets was applied over the storefronts.
- c. One-story, commercial or professional buildings.
- d. Structure is less ornamented, with most of the detailing contained in storefronts and windows.
- e. Storefront glazing is maximized to show off merchandise. Infill below the windows is usually of wood, as are sash members and doors.

Some of the mid-block buildings have been constructed more recently and constructed of poured concrete or concrete block rather than brick. They lack the detail of the older buildings both in structural detailing and storefront interest. They do, however, provide visual continuity to Main Street. These buildings were built with economy in mind and are the second generation of structures in Buckley, built after the fire of 1925. They should be viewed as "background" buildings taking a back seat to their older, more ornate neighbors, and should not be ornamented to imitate them. It should, however, be noted that their infill function makes them an important part of downtown.

- a. Single story, commercial occupancies.
- b. Concrete or concrete block construction.
- c. Very simple lines with no ornamentation.
- d. Metal storefronts and doors that have replaced the original wood systems.

3.2.3 Miscellaneous Commercial Buildings:

The District contains several miscellaneous buildings with more individual characteristics. They exhibit a variety of roof forms, more variation in materials and vary greatly in age and use. Because yards and setbacks are introduced for parking, landscaping or service, these structures serve as a natural buffer and transition to the residential neighborhoods that surround the commercial center.

a. Former Methodist Church and Mortuary, now vacant:

A dignified wood frame structure which was moved to its present site many years ago. Its history and formality earn it a place on Main Street.

b. Former Gas station at "A" Street and Main:

This type of gas station dates back to the forties and is becoming rare. The scale of the service bays, windows and roof forms make it a good

transition to the residential neighborhoods to the east.

c. Cafe, Offices and Laundromat on north side of Main:

These buildings provide some mid-block infill but are of such different materials from all other buildings in town that they should be dealt with individually as "background structures".

d. City Multi-Service Center:

On the site of the old Buckley Hotel, this one story brick and wood public building was built in the seventies and was an attempt to reflect Buckley's rural setting. It is an example of roof forms, materials, building siting and scale that do not contribute to the traditional personality of Main Street.

The architectural character of each shall be identified and maintained, with renovations planned to be true to the original construction, and subject to Architectural Review Board approval.

Buildings that lack strong character shall be renovated to compliment the rest of the Downtown District without resorting to gimmicks or false ornamentation.

3.3 EXISTING STRUCTURES

Restoration and renovation of Downtown Review District buildings shall be carried out in accordance with the following guidelines and as directed by the Architectural Review Board.

3.3.1. Masonry:

- a. Non-original materials such as wood, metal or plastics are not acceptable substitute materials for original masonry walls.
- b. Clean brick and stone with water and non-abrasive cleaners that will not erode or change the surface of the material. Sandblasting is not acceptable.
- c. Repoint brick and stone joints to match original grout profile. Repair cracks and replace non-conforming masonry work or other non-masonry materials with brick or stone that matches the

color and dimensions of original materials.

- d. Most brickwork in Buckley was originally unpainted. Whenever possible, unpainted conditions should be restored. Use of high quality, low sheen sealers is acceptable. If repairs to brickwork are extensive or matching materials are not available, painting of brick may be an acceptable alternative.
- e. Perform seismic bracing without damaging or destroying original materials. The introduction of bolts and new structural supports on the building's exterior should be avoided or carefully planned.
- f. Repair masonry cornices to original profiles. Substitute materials may be acceptable to achieve original visual appearances if structural or other construction constraints do not allow recon-

struction in masonry.

3.3.2. Concrete or Stucco Exteriors:

- a. Patch and repair as required. Remove any non-original materials such as wood or metal siding. Repaint to compliment natural brick structures.

3.3.3. Wood Storefronts and Detailing:

- a. Restore wood cornices and storefronts to original condition using historic photographs as a guide. Much of the character of these buildings lies in the quality and proportions of the carpentry work, and much pedestrian interest can be created through proper restoration.

3.3.4. Wood Windows:

- a. Windows of this building type are typically taller than they are wide, in a 2:1 or 3:1 ratio. Upper transom panels are broken into individual lites that are vertically

oriented. These proportions as well as the size and thickness of sash and mullion members are to be maintained or restored to original appearance.

3.3.5. Metal Cornices and Ornament:

- a. Repair or replace building cornice work with new materials, basing profiles on photographic evidence available. Original stamped metal may be replaced with stamped aluminum or fiberglass castings made from remains of any existing cornice fragments. Paint restored cornices and ornament to match original, basing color selections on shades and tones visible in photos, and on evidence of color preferences known from similar buildings.

3.3.6. Cast Iron Structure and Storefront:

- a. Restore or replace all cast iron work to match original profiles. Fiberglass or aluminum

castings are acceptable substitutes for cast iron that is missing or beyond repair. Paint new work to replicate historic conditions, as based on photographic evidence and similar built examples from adjacent structures or other cities.

3.4 NEW CONSTRUCTION

New structures built within the Downtown Review District shall be constructed to compliment existing buildings, but should not attempt to imitate or literally copy them. Setbacks, roof forms, building heights and general massing shall be in keeping with the existing structures on either side of the proposed site.

New buildings constructed on Main Street shall be designed to reinforce the traditional pattern of two story corner structures and one story mid-block buildings.

Doors, windows, details and ornament shall be of similar materials, scale and proportion

to those elements found on existing buildings and described elsewhere in these guidelines.

3.5 UNIFYING ELEMENTS

- 3.5.1 Signage: Signage in the Downtown Review District is to be designed with pedestrians in mind. The size, scale, color and lettering of messages shall be appropriate to walking and slow vehicular speeds, and be reminiscent of original Main Street signage as found in photographic documents.
- a. All new signs and all changes in existing signs visible to the public shall be submitted to the Architectural Review Board for approval. Signs shall be reminiscent of the styles and materials available at the time the building was constructed. The ARB shall take into consideration the skill and technical quality of signs submitted for approval and may rescind approval

for any sign not executed in accordance with materials submitted for review.

- b. A maximum of fifty square feet of total signage is allowed for each storefront, including wall signs, window signs and both sides of projecting signs, first and second floors.
- c. Signage shall not cover architectural ornamental features, unless originally a feature of the building.
- d. Signs and individual letters shall be sized appropriately for slow moving traffic and pedestrians.
- e. Projecting signs shall be no larger than nine square feet per side.
- f. Signs on adjacent storefronts shall be coordinated in height and proportion and designed to unify the street elevation.
- g. Rear entry signs shall be no more than four square feet.
- h. Projecting signs and signs placed flat against the building wall are encouraged. Projecting signs shall

be compatible with the period of building to which it is affixed, and shall harmonize with the lettering and symbols chosen for it.

- i. The following sign types are not allowed:
 - 1. Portable readerboard signs at ground level,
 - 2. signs with moving elements unless specifically approved by the ARB,
 - 3. signage on awnings that are internally lighted and not of historical design,
 - 4. new billboards or rooftop signs.
- j. The restoration and preservation of historic wall and architectural signage is encouraged. No new signs shall be installed or painted in place of historic signage.
- k. Lettering: Signs shall be professionally lettered and display a level of craftsmanship appropriate to the downtown district. Lettering shall be of a traditional block

or curvilinear style which is easy to read and compatible with the style of building. Generally, no more than two different lettering styles should be used on one sign. Examples of acceptable lettering fonts are included in these guide lines.

- m. Construction shall be of metal, wood or other durable material as approved by the ARB and appropriate to the building on which the sign is installed.
- n. Color: Signage colors shall be appropriate to the era in which the building was constructed and in all cases be compatible with the predominantly red brick facades of downtown buildings.
- o. Lighting: Signage in the downtown district shall be unlighted or lighted from external sources. Neon signs may be acceptable to the ARB if custom-designed to be compatible with the building's historic or architectural features.

- p. Signage structure shall be designed to compliment the sign, not clash or overwhelm it. Attachments to building materials shall not permanently damage any architectural ornament or feature, and shall avoid damaging bricks or masonry.

3.5.2 Awnings:

- a. Awnings have been a traditional addition to the facades of buildings downtown and shall be encouraged as a unifying exterior feature.
- b. All awnings shall be compatible with the historic character of the buildings on which they are placed and shall be based in design upon historic counterparts. They shall reflect the architectural style and form of the building and shall be, or appear to be, retractable in keeping with historic precedent. They shall be attached to the building in a manner that does not

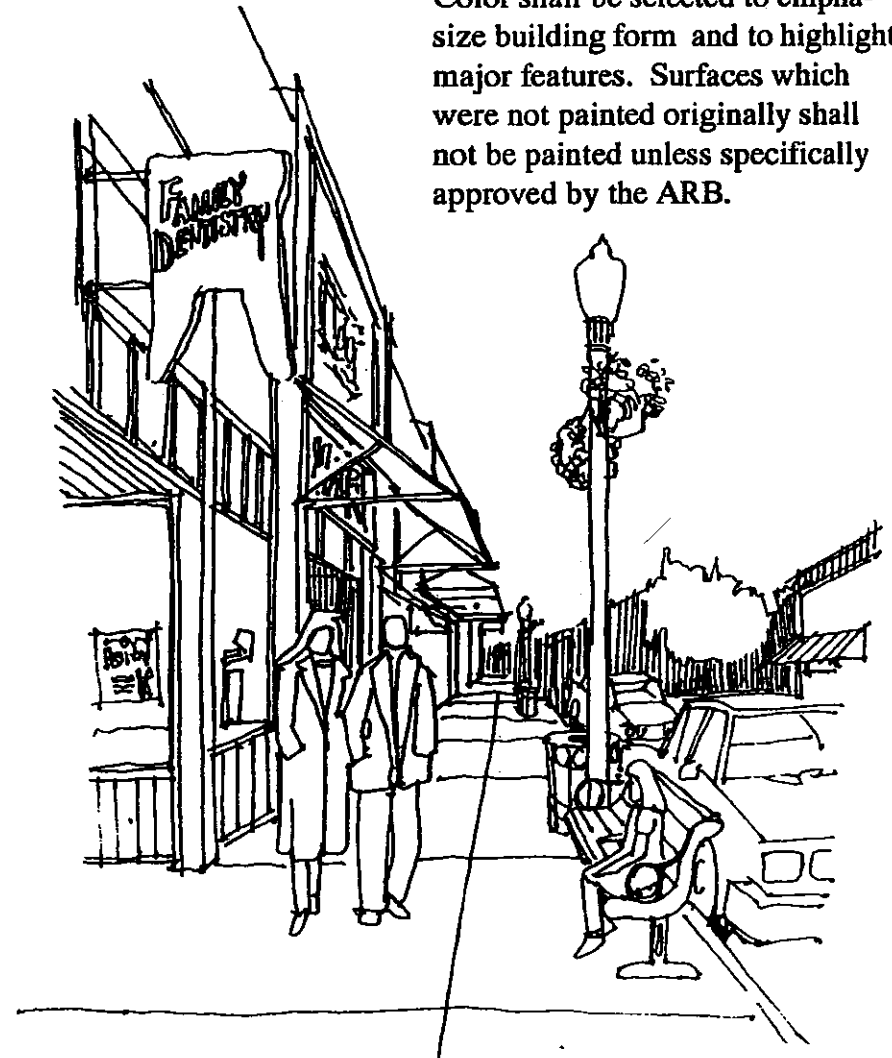
permanently damage the structure or obscure significant architectural features.

- c. Construction shall be of canvas-like materials in traditional textures and sheen. Back-lighted or translucent awnings are not allowed.
- d. Awnings shall be in colors and/or patterns which compliment the building and have basis in the historical record.

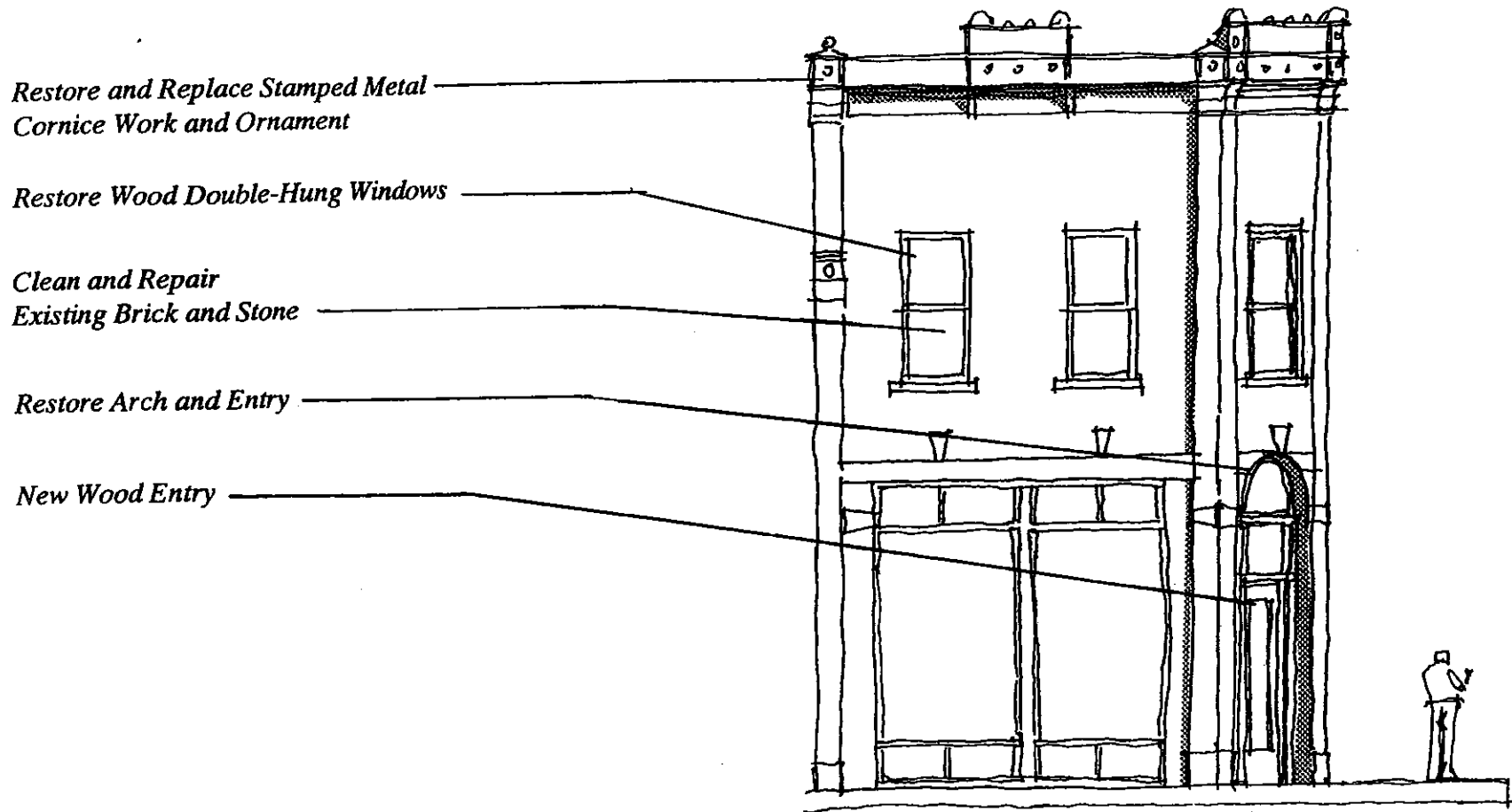
3.5.3 Building Color: Color for existing buildings in the Downtown Review District shall approximate the original colors of materials and finishes. Materials may be scraped to provide evidence of former schemes, or written or anecdotal evidence used to establish an authentic color palette. Since color is subjective, however, the original color may have been inappropriate and painted over. Discretion should therefore be used in the selection of any new color scheme. Several

national paint companies have introduced colors which have historic origins. Review of historic colors appropriate to the building's age can be a good method of selecting a scheme. Any colors selected should be judged for their appropriateness to use in the City of Buckley.

All color schemes shall be reviewed and approved by the ARB prior to application. The ARB may require sample applications of paint colors on site before final approval. Building colors shall contribute to the architectural character of the building and be considered in combination with signage, awnings and other elements. Original building colors shall be considered for any existing building renovation. For new construction, colors should be chosen that are muted and work well with the overall red brick color of most downtown buildings.



Color shall be selected to emphasize building form and to highlight major features. Surfaces which were not painted originally shall not be painted unless specifically approved by the ARB.



Example: Corner Building : 711 Main

*Clean and Tuckpoint Existing Brick.
Restore Detailing*

*New Wood Double-Hung Windows
on Second Story*

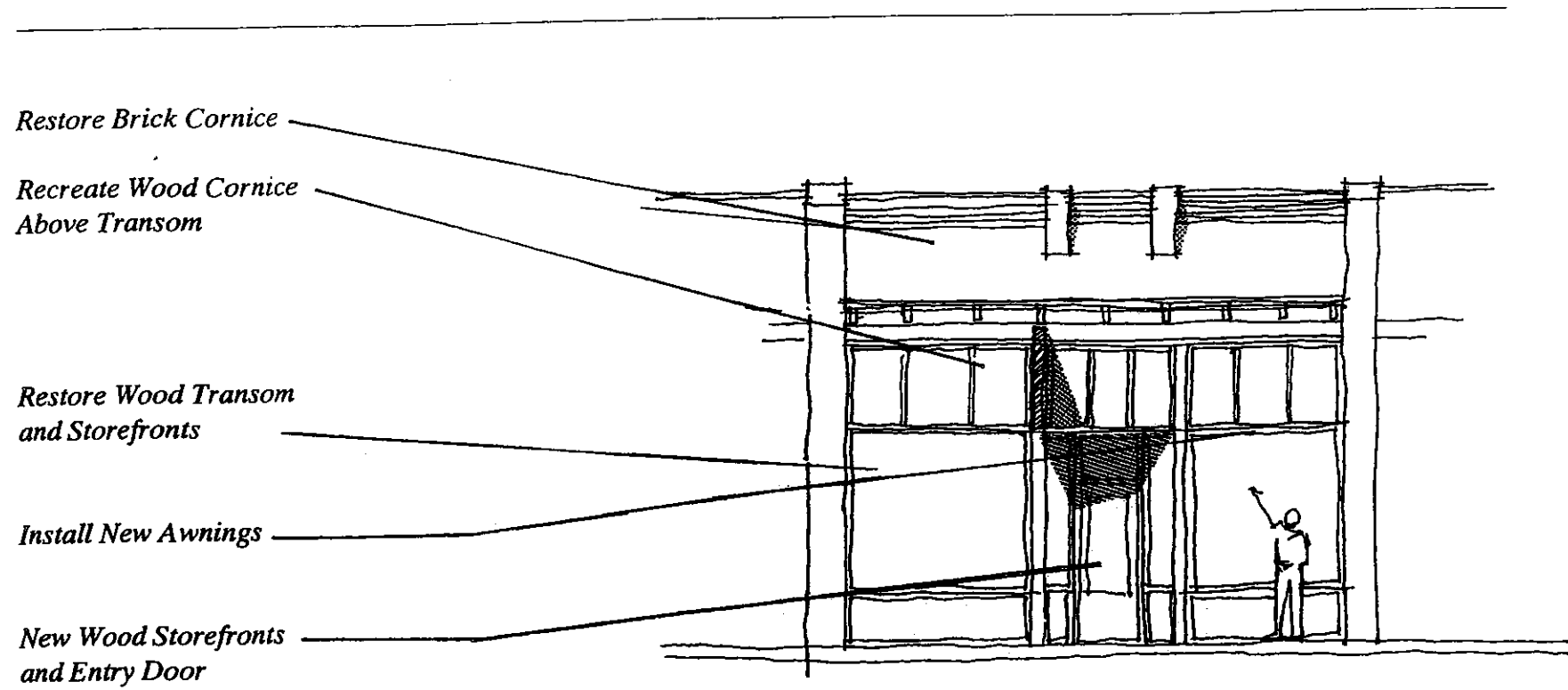
*Install new Balcony on Wooden or Cast
Iron Posts*

*Restored Wood Storefronts
with Transom Windows*

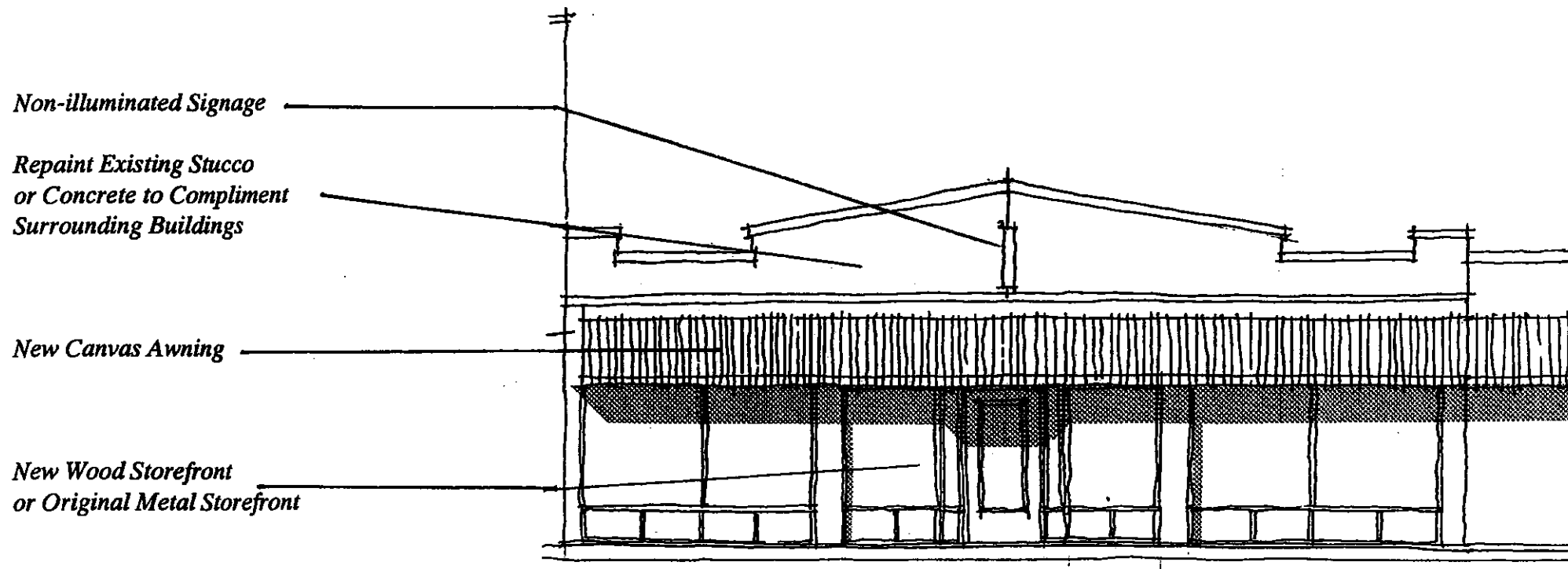
*Restore Cast Iron Column Accent
on Corner*



Example: Corner Building: 780-790 Main



Example: Typical Mid-Block Building



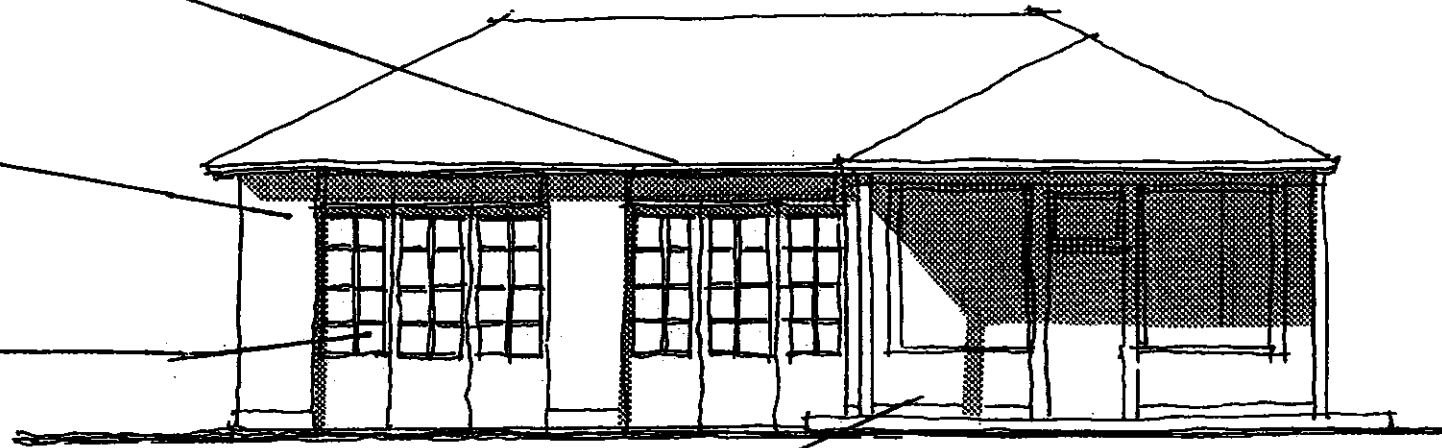
Example: Newer Mid-Block Building

*Signage Locations
over Service Bays*

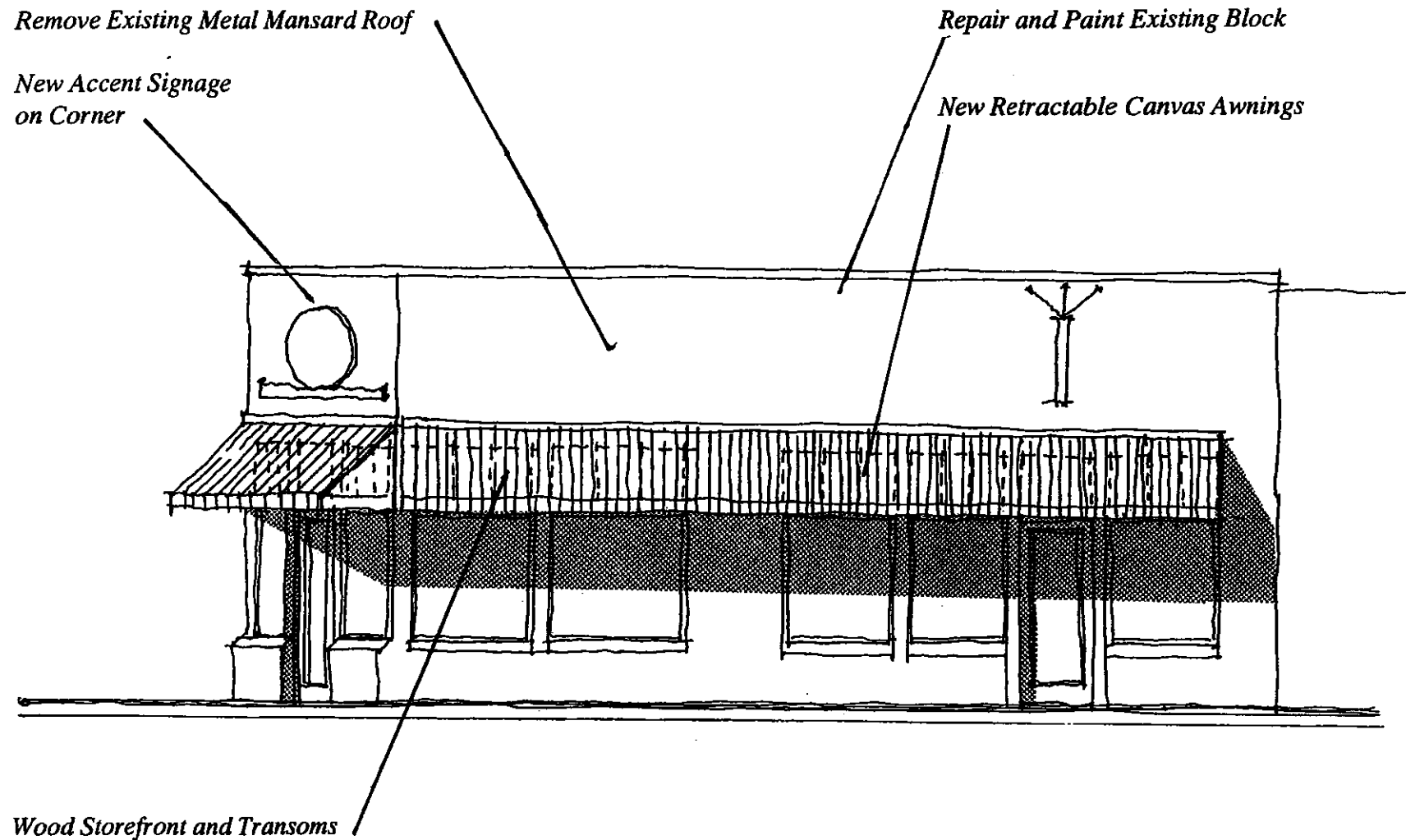
*Repaint and Restore
Structure to Original
Appearance*

*Restore Original
Service Bay Doors*

*Creatively Reuse Existing
Architectural Elements for
New Purposes.
Canopy Becomes Outdoor
Seating area or Retail Display Space*



Example: Miscellaneous Commercial Buildings



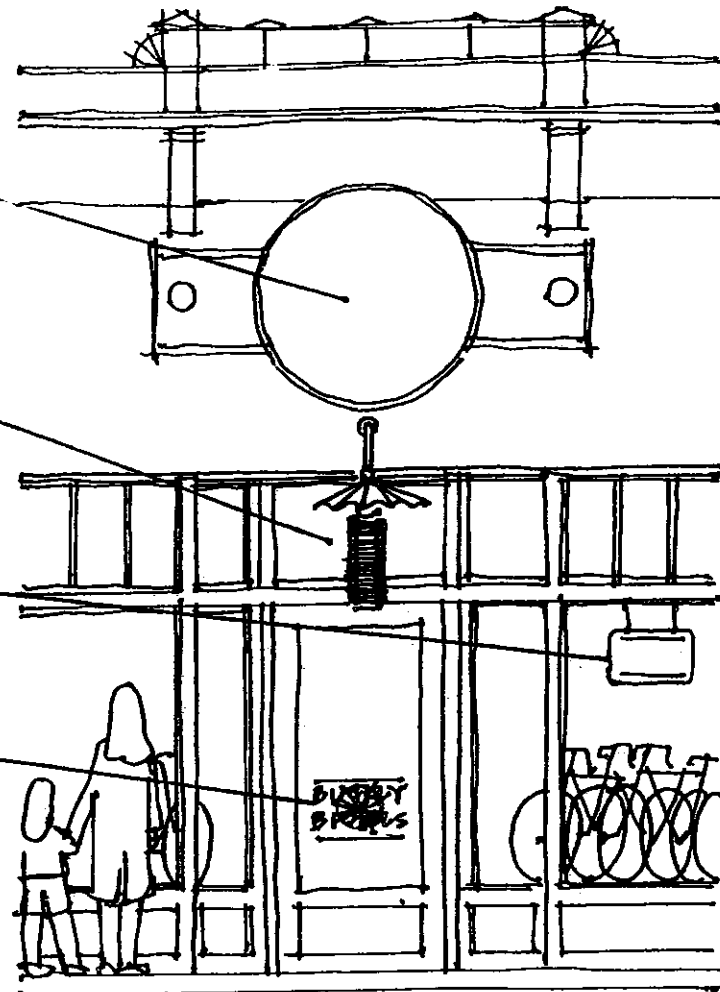
Example: Miscellaneous Commercial Buildings

Flush mount painted Wall Signage:
Gloss enamel on wood or metal.
Unlit (shown) or externally lit.
Vehicular scale.

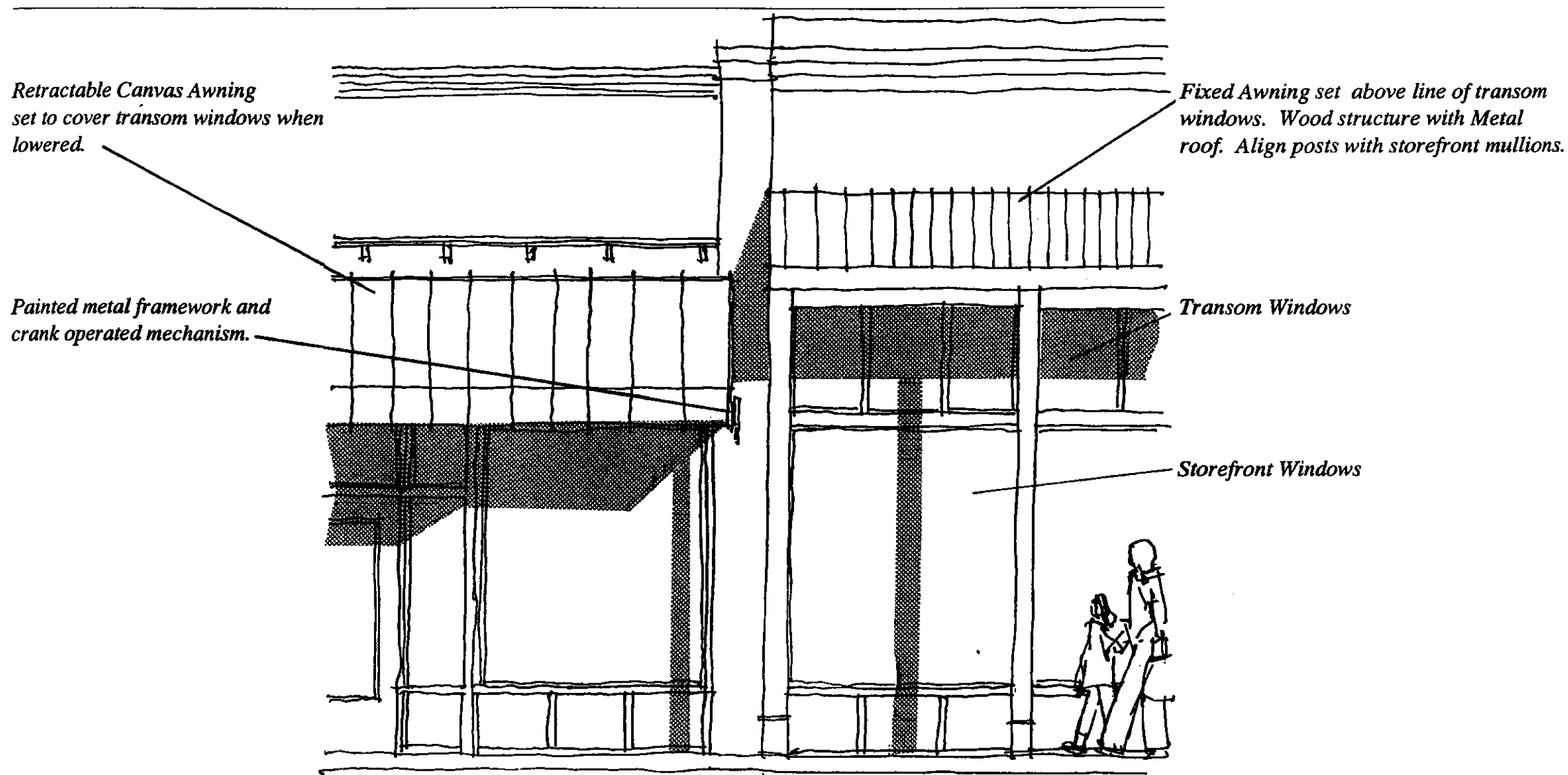
"Blade" Sign extending perpendicular
from building face. Gloss enamel on
wood or metal. Externally lit from above
(shown) or side, or unlit.
Pedestrian scale.

Internal window signage. May be
internally lit, neon or unlit, depending on
age of building with approval of ARB.

Painted window signage. Opaque gold
leaf or colored messages and graphics.
Pedestrian scale.



Downtown Commercial Signage



*Retractable Canvas Awning
set to cover transom windows when
lowered.*

*Painted metal framework and
crank operated mechanism.*

*Fixed Awning set above line of transom
windows. Wood structure with Metal
roof. Align posts with storefront mullions.*

Transom Windows

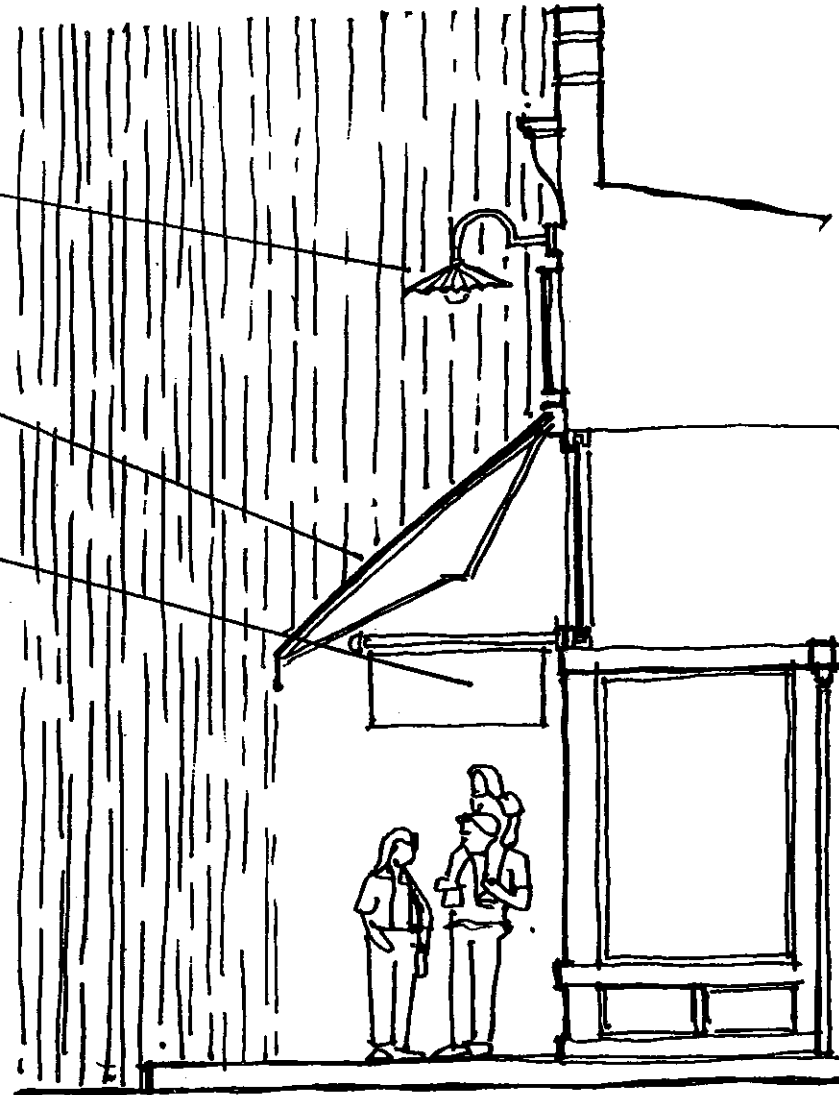
Storefront Windows

Awnings

*New Surface Mounted
Signage Lighting*

Retractable Canvas Awning

*Projecting Sign under
Awning*



Awnings

New Street light:

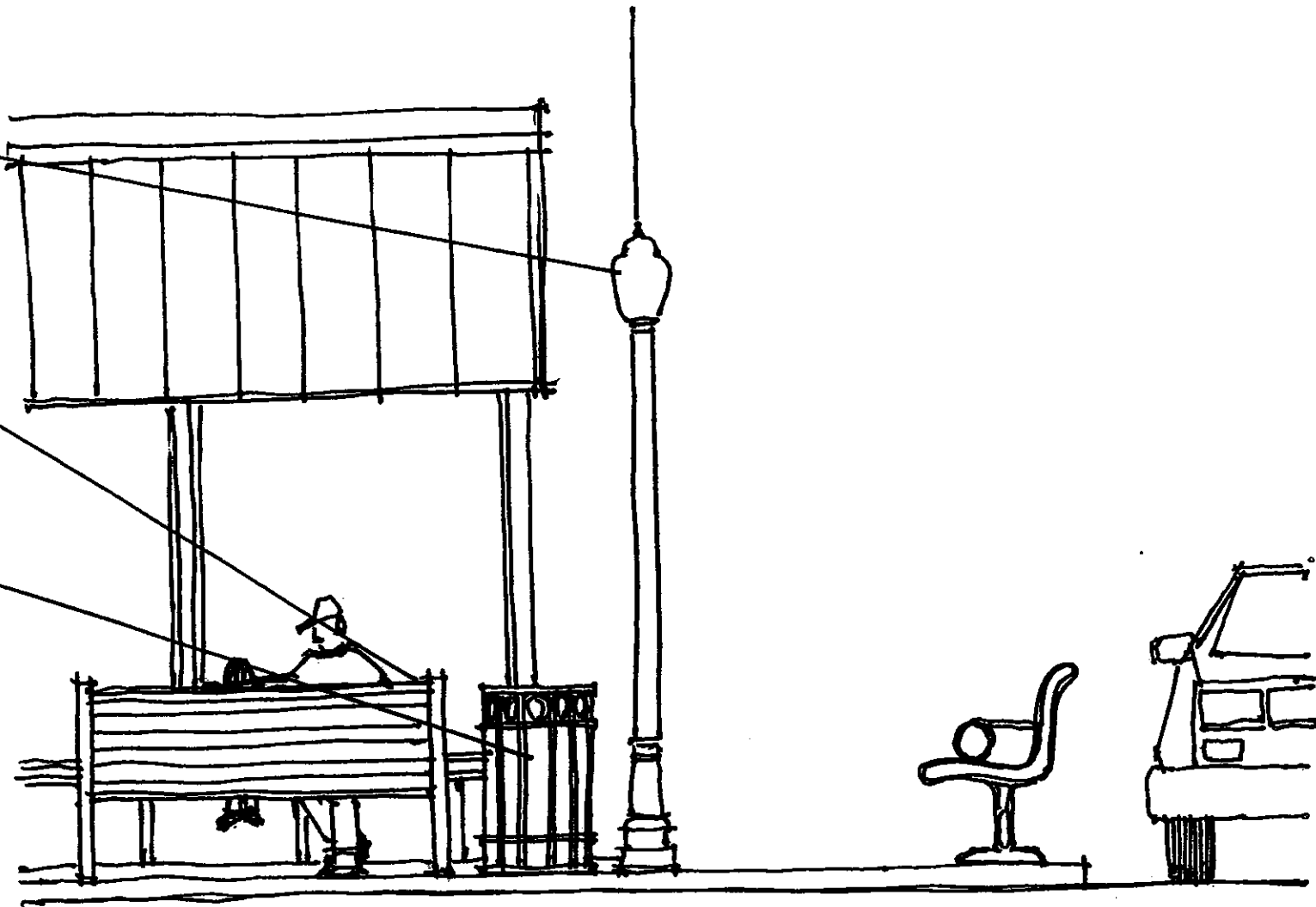
*Clear textured polycarbonate
"Acorn" Globe mounted on painted
Cast Aluminum Post, metal halide lamp*

Bench:

*Painted cast aluminum with
wood seat. 6 foot length, bolted
to sidewalk.
Canterbury International model 700
"Embarcadero" shown.*

Trash Receptacle:

*Painted cast aluminum with liner.
23 inch diameter, 36 inches high.
Canterbury International "Pennsylvania
Avenue" model shown.*



Street Furniture

4. Highway 410



4.1 INTRODUCTION AND SCOPE:

While the downtown review district represents much of Buckley's past, defining its current image and character, the land along Highway 410 will contain much of Buckley's future. As the city and region grow this open, level land will be increasingly converted from agricultural and low density development to commercial and industrial uses. How this change is allowed to take place will in large measure influence the character and quality of life for the citizens of Buckley into the next century.

The Highway 410 corridor provides visitors and passers-by with their first and sometimes only impression of the city. It should be considered Buckley's front yard. A favorable impression along the highway increases the potential for visitors to turn into Main Street for further exploration of the community.

As previously stated, the goal of downtown redevelopment is the restoration and reha-

bilitation of the current building stock to its original appearance. The introduction of unifying elements such as signage, street furniture and awnings help tie together the individual properties and create a sense of place. This reinvestment will allow the downtown area to continue as a commercial center for Buckley.

The design issues associated with Highway 410, however, are different ones. Here, the goal of these guidelines is to carefully plan for the conversion of residential and farmland to new commercial and industrial use. Buildings will be generally of new construction. If Buckley is to continue as a City with an individual identity it is critical that these new facilities be sympathetic to the surrounding landscape and visually support the existing downtown core. It is not appropriate, to introduce new flat-roofed brick buildings along the highway, however, because the conditions of development and patterns of use are different.

Downtown traditionally developed as a

place for pedestrians and slow speed horse (and later automobile) traffic. Parking is along the street, buildings make use of common walls and are as close together as possible to minimize the walking distance between small shops.

In contrast, new development on the highway will be larger in scale, flanked by large parking areas, approached by car, and viewed at relatively high speeds. A variety of uses must share the area; large retailers, manufacturers and higher density residential developments. Unlike the downtown area, new buildings on the highway will function and be seen as individuals, not connected by shared walls and facades. These buildings must take a different form that suits their purposes without destroying the character and individuality of Buckley.

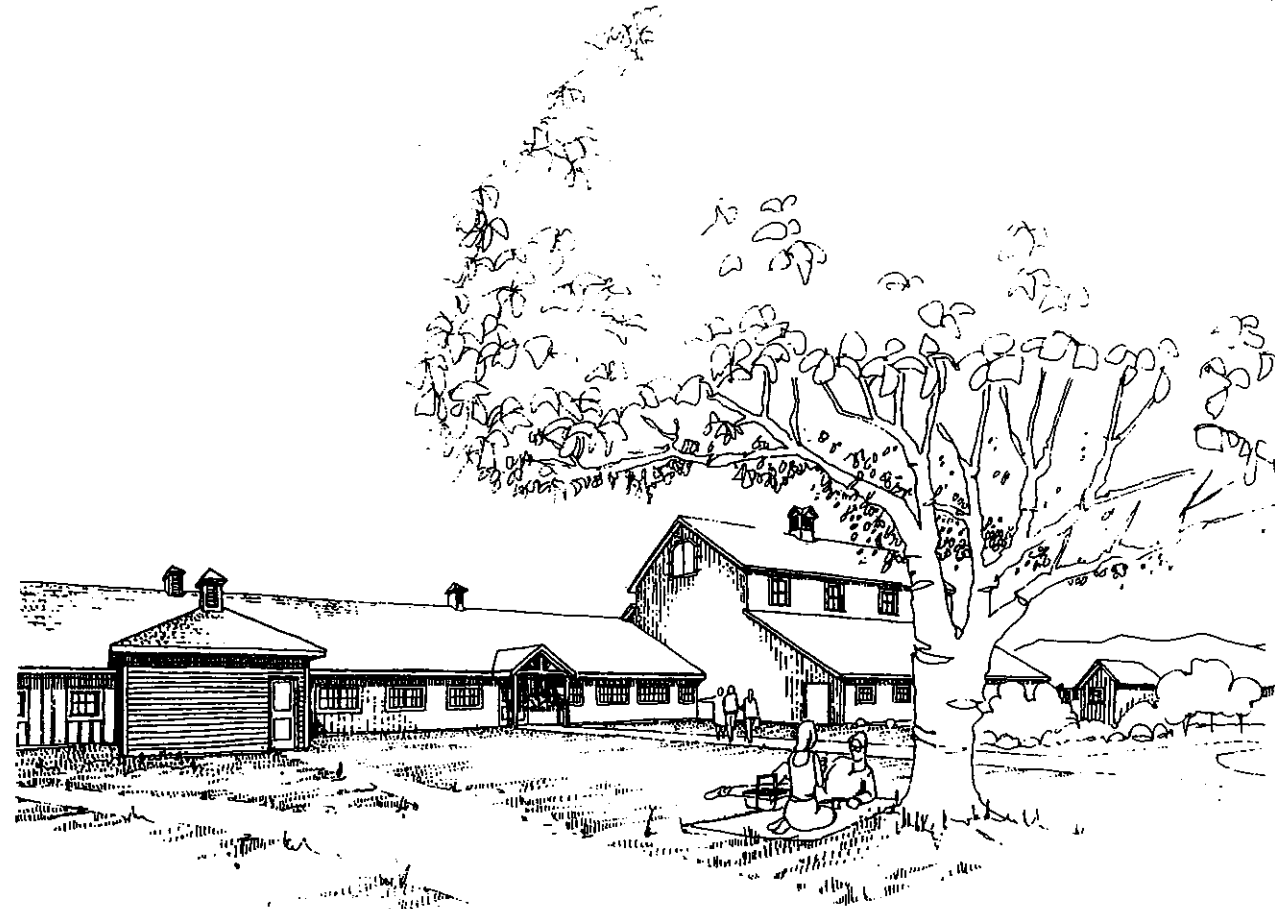
The Buckley plateau and surrounding hills contain many fine examples of architecture built by settlers and those who came after them. These barns and lumbermills look at home in the landscape because their forms

are simple and straightforward, accomplishing their purposes without gimmicks or unnecessary ornament.

Construction is principally of wood and metal, with long, horizontal lines and sloping roofs. These large storage and work buildings with long roof spans and tall ceilings, make appropriate and natural models for new construction along Highway 410.

The following guidelines for development in the Highway 410 Review District are based therefore on the creative and sensitive reintroduction of these traditional building materials and forms to new commercial and industrial uses. The guidelines also require unifying elements for the overall development of properties along Highway 410 in designated review areas.

New developments and renovation of existing buildings and properties serving current and new uses are required to comply with these guidelines as interpreted by the ARB as a part of the permit process.



4.2 BUILDING CONSTRUCTION:

Buildings and new development along Highway 410 should be designed to reflect and reinforce the rural character of the area. They should be reminiscent in form, material and detail of the traditional types of buildings in rural Pierce County, and particularly the Buckley Plateau. Traditional structures were primarily built here for the agricultural and timber industries, and they are straight-forward and functional in appearance.

4.2.1 Site Development:

Projects within the Highway 410 Review District shall be designed in compliance with all City of Buckley, Pierce County and Washington State planning and development requirements. In addition projects subject to review shall include:

- a. A landscape buffer of thirty-five feet in width (or as approved by Architectural Review Board) from property line shall be installed along the Highway 410 frontage.

This buffer shall include:

1. An irrigated planting strip no less than ten feet wide with shade trees of species and spacing approved by the ARB.
2. A six foot wide concrete meandering pedestrian path meeting City of Buckley requirements.
3. A bermed parking area separation no less than fifteen feet wide and three feet high with irrigated landscaping as approved by the ARB. Contained within this buffer shall be allowable development signage and City standard street lighting at intervals as determined by the City Engineer.

Parking areas shall include no less than five percent irrigated landscaping area, exclusive of street buffer landscaping. This landscaping shall provide islands of green space to separate parking into smaller areas and provide a useable amenity. Landscaping shall be uniformly dispersed across the

project site.

Development proposals shall be designed to coordinate with adjacent properties so that greenspaces, landscape buffers, pedestrian paths, lighting and signage serve to integrate individual developments.

4.2.2 Scale and Form:

Projects shall relate to the scale and form of traditional agricultural and mill buildings. Traditional buildings often were added onto over the years. Farms and mills usually consisted of a cluster of similar buildings. New construction shall utilize these devices to reduce the scale and apparent mass of larger projects.

- a. Long, uninterrupted buildings and developments are not acceptable. Modulation of the building facade is required by shifting the elevation no less than four (4) feet every eighty (80) feet of building length.
- b. Individual buildings or developments shall not exceed eighty (80) feet long, or 20,000 gross square

-
- feet in area.
 - c. Individual buildings within overall developments shall be separated by no less than 25 feet of landscape buffer space. Covered walkways may connect across buffer spaces.
 - d. The architectural design of new projects shall take into consideration and respond to the scale and character of the rural landscape.
 - e. Overall architectural design shall be based on traditional agricultural and mill buildings.
- 4.2.3 Architecture and Structure:**
- a. Where visible from the exterior, wood truss and heavy-timber structural systems are recommended.
 - b. Buildings shall be no more than two stories and shall have sloping roofs with a minimum pitch of 8 in 12 unless otherwise specified by the ARB.
 - c. Roof forms shall incorporate
 - simple gables and sheds with generous overhangs.
 - d. Siding: Utilize horizontal beveled wood, wood shingle or vertical wood board and batten siding whenever possible. Where required by building code or for functional reasons, concrete and concrete masonry materials may be employed with approval of the ARB. In such cases concrete and concrete masonry shall be detailed to emulate traditional stone construction.
 - e. Roofing: Wood shingle or galvanized metal roofing shall be employed on all new construction unless otherwise approved by the ARB. Existing rural buildings have often replaced these materials with composition roofing, and some types of composition roofing, particularly patterns that reflect a wood shingle appearance may be acceptable for new projects.
 - f. Doors: Wood doors shall be simple in shape. Large openings may be detailed to resemble traditional barn doors intended to accommodate animals and machinery.
 - g. Windows: Windows shall be fixed, casement, awning or double hung, with a square or vertical orientation. Sliding windows are not acceptable. Wood or metal sash materials are acceptable. Break large areas of glass into smaller lites whenever possible.
 - h. Ornament: Ornament shall be minimized to reflect the utilitarian nature of the buildings. Achieve architectural interest through structural elements such as gable brackets, posts and beams. Stress craftsmanship and the detailing of ventilators, cornerboards.

4.2.4. Color:

Colors should generally be muted, and simple painting schemes were typically used in these traditional buildings. Color schemes employing many colors or particularly bright colors shall be avoided. Natural, unpainted wood with a clear sealer, or wood painted grey, dull red or green are examples of acceptable color palate.

Colors should be selected to emphasize building form and highlight major features. The use of brightly colored or glossy building materials such as metal or tile siding or roofing are not acceptable.

4.3 UNIFYING ELEMENTS

4.3.1. Signage: Allowable signs in the Highway 410 district are divided into two types:

- a. Monument Ground Signs identifying a building or complex. These are large, direct signs that can be identified and understood at highway speeds from some distance away. The goal of this signage is to communicate a simple message clearly and in a manner that does not detract from the landscaped development of the highway.
- b. Signs attached to a building that identify the tenants and activities at a pedestrian oriented scale. These signs serve a similar purpose to the signage along downtown and shall be similar in design.

4.3.2. Monument Ground Signs: Signs identifying individual business buildings or multiple-tenant complexes shall be coordinated with the City of Buckley's streetscape concept for

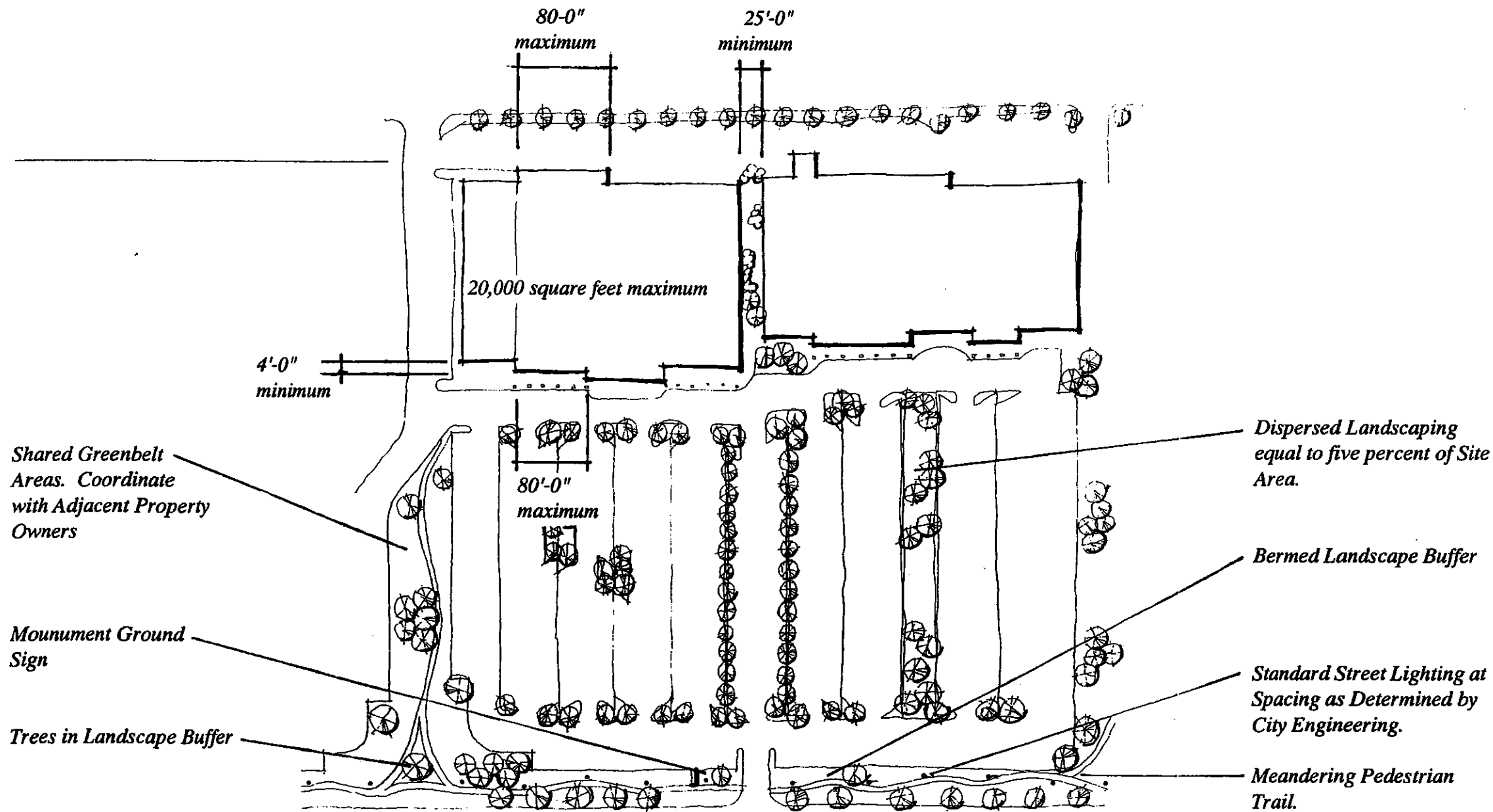
- the Highway 410 frontage.
- a. Each building or complex is allowed one freestanding sign per street frontage of not more than one hundred (100) square feet, inclusive of both faces and supporting structure. These signs shall be no more than five (5) feet high.
- b. In addition, each commercial parking lot is allowed one identification or directional sign per street frontage not to exceed six (6) square feet in area and five (5) feet in height.
- c. All such signs shall fall within the required bermed landscape buffer along Highway 410.
- d. Signage along the Highway frontage shall be unlighted, lighted from commercial grade recessed ground fixtures set in concrete or internally lighted.
- e. Sign body construction shall of concrete, wood or metal set on

a concrete or masonry foundation or base. Acrylic faces are permitted if sign is internally lighted. No moving elements or flashing lighting is permitted.

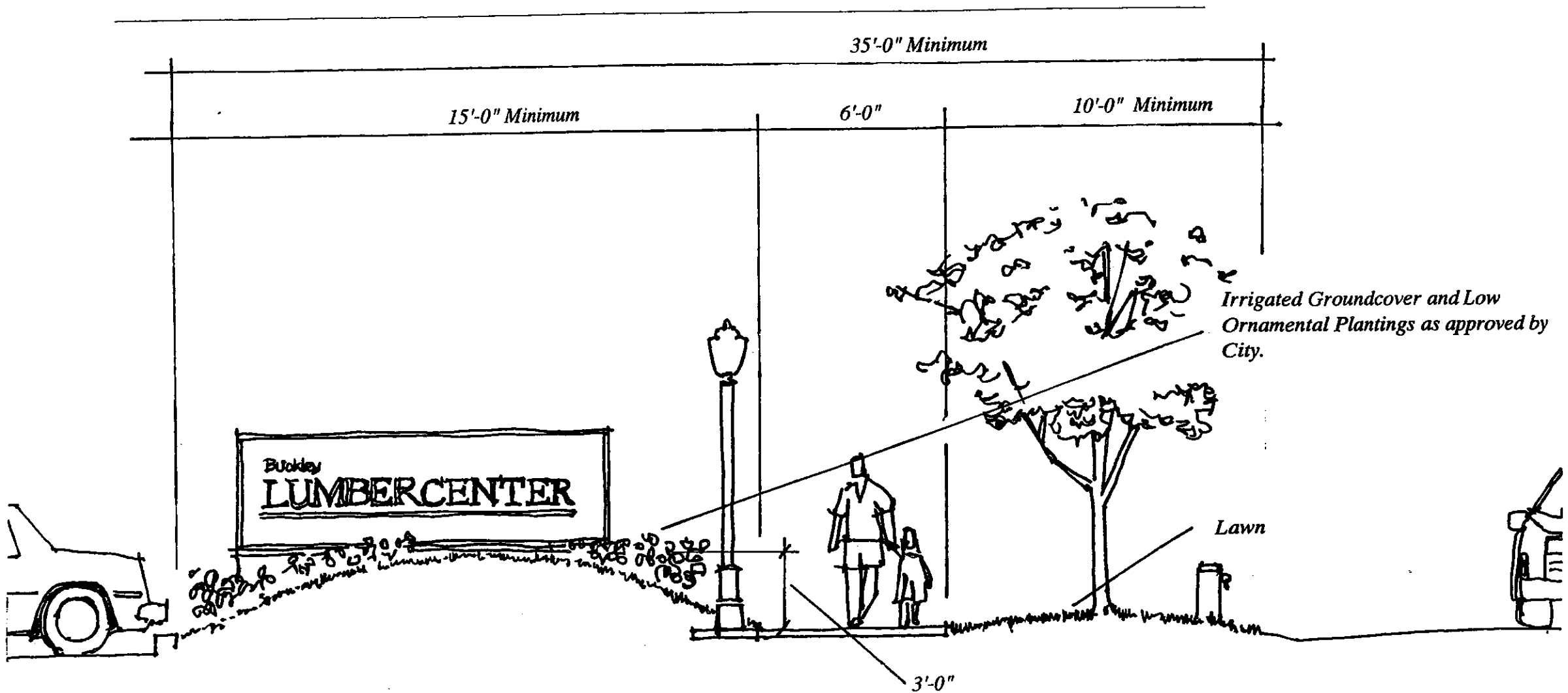
4.3.3. Building Mounted Signs:

- a. Regardless of street frontage, each individual business shall be permitted no more than one hundred (100) square feet of sign area.
- b. Signs installed in windows shall be considered part of the total sign area allowed for that business, except that credit card information and normal hours of operation shall be excepted.
- c. Wall signs may be flush with or projecting from the building face. They may not cover more than seventy (70) percent of the lineal frontage of the wall.

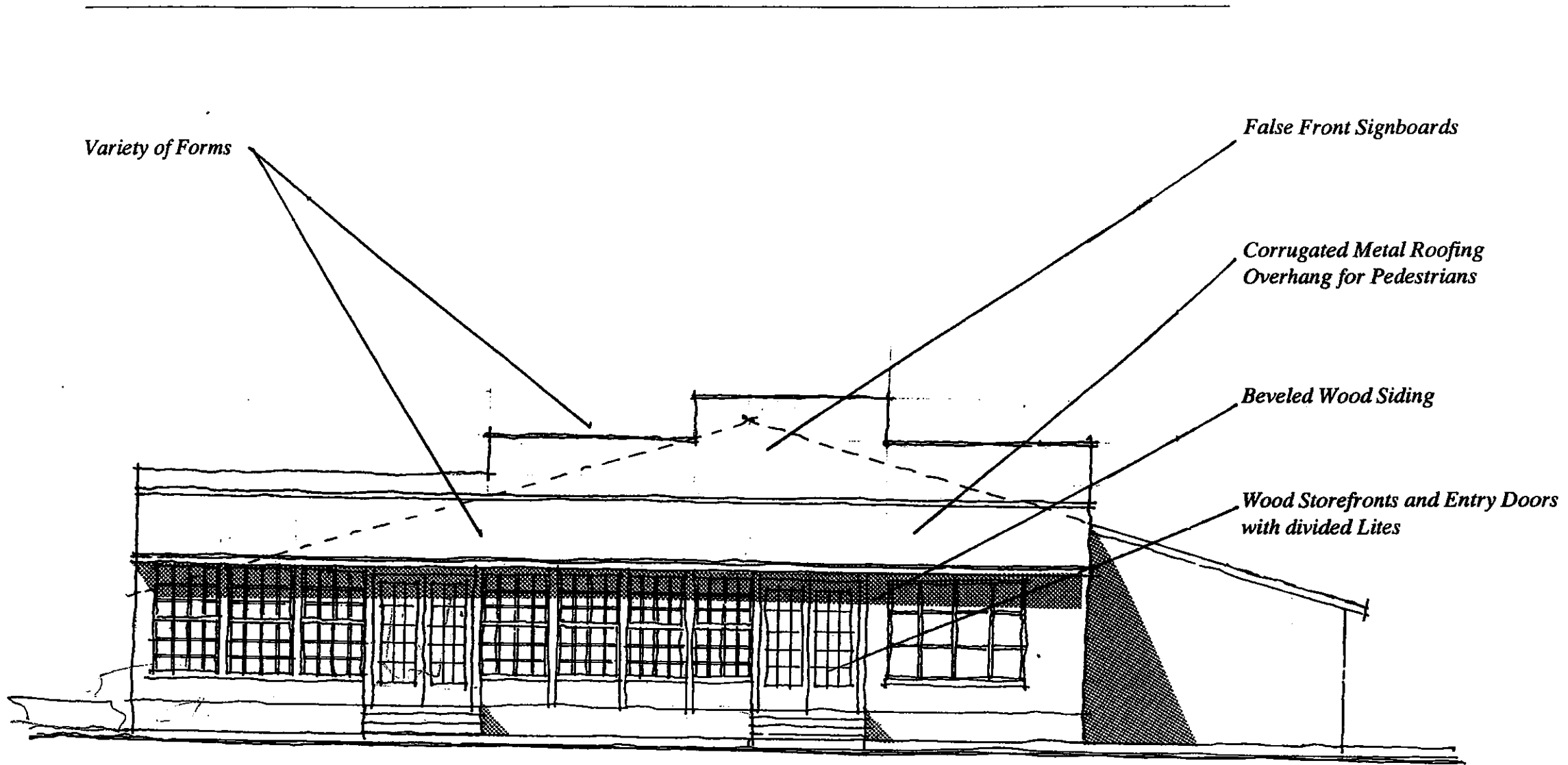
- d. Projecting signs may not extend more than four (4) feet from the building and may not exceed sixteen (16) square feet in area.
- g. The following signage is not permitted:
 - 1. Back-lighted or translucent awnings with messages.
 - 2. Internally lit surface-mounted "box" signs.
- h. Each business within a multiple-tenant building shall be considered as a separate business for signage purposes and is permitted wall signage not to exceed a maximum of thirty-two (32) square feet.



Example: Commercial Site Development



Highway 410 Buffer Development



Example: Single Story Retail

Wood Shingle or Metal Roofing

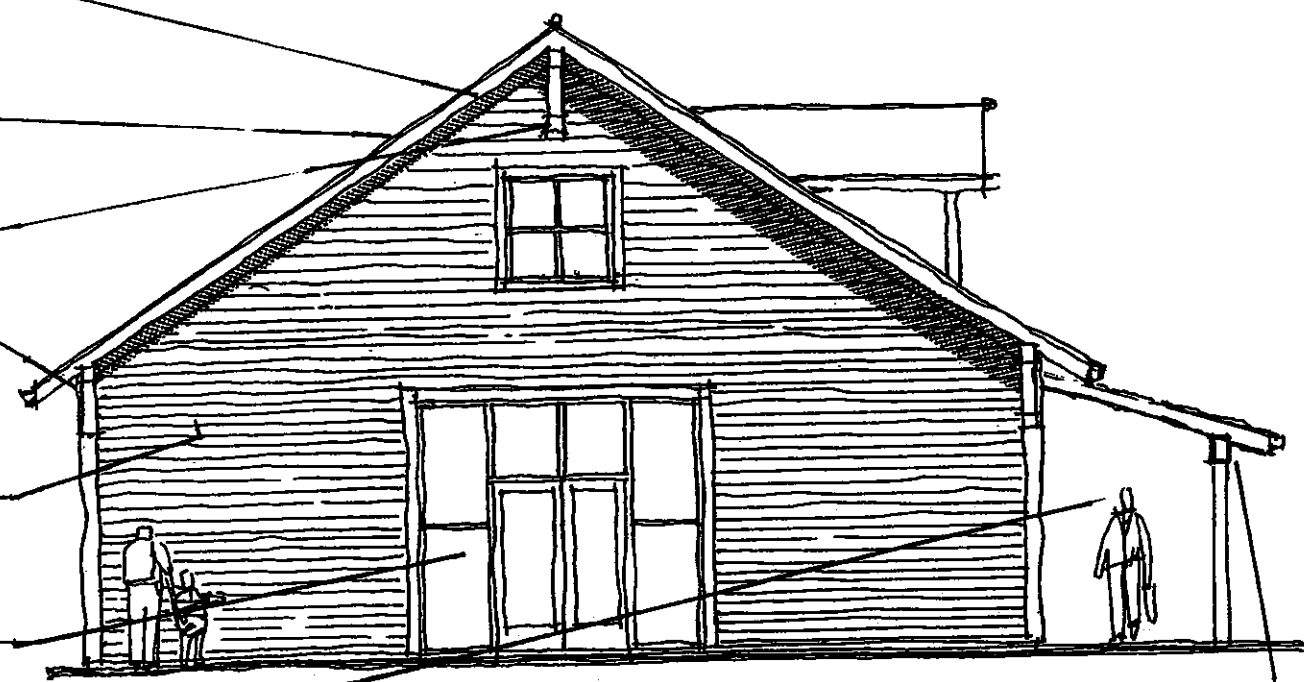
8 in 12 Pitch or Steeper

Eave Brackets

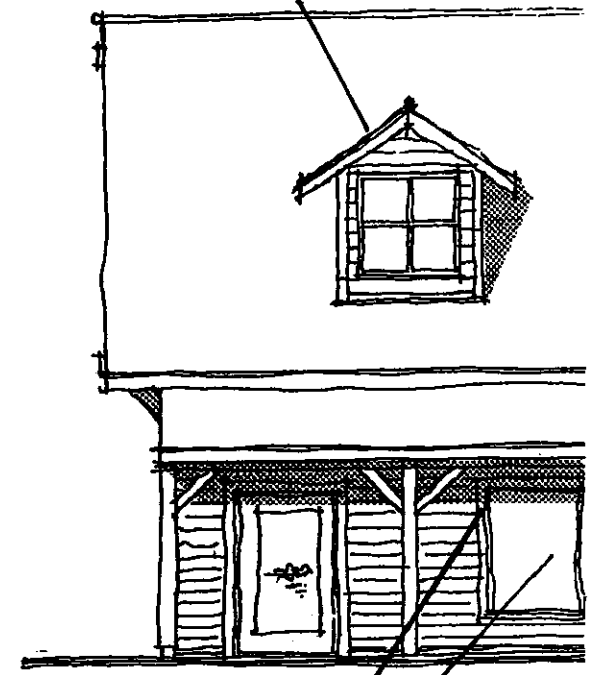
Bevel or Shingle Siding and Corner Boards

Wood Storefront Replicating Barndoor Infill

Lean-to Pedestrian Shelter with Bracketed Posts



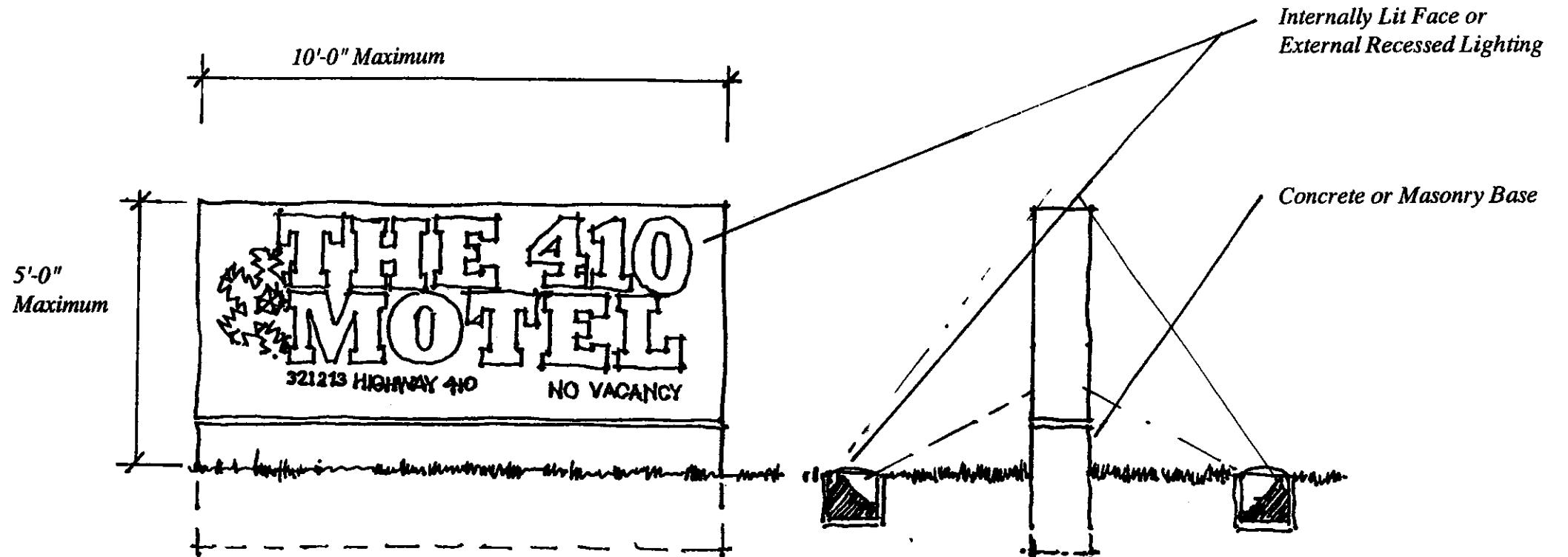
Dormers for Upper Offices



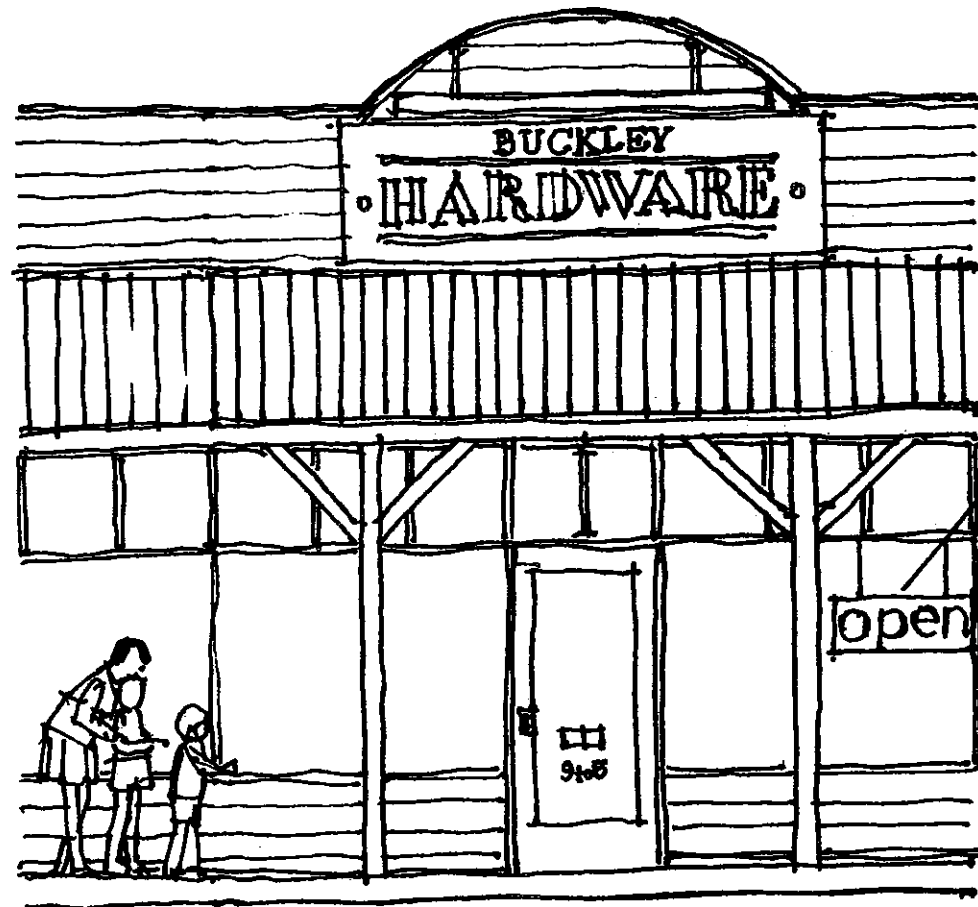
Individual Windows and Doors Repeated Regularly

Lean-to Pedestrian Shelter with Bracketed Posts

Example: Two Story Retail/Office



Highway 410 Freestanding Signage

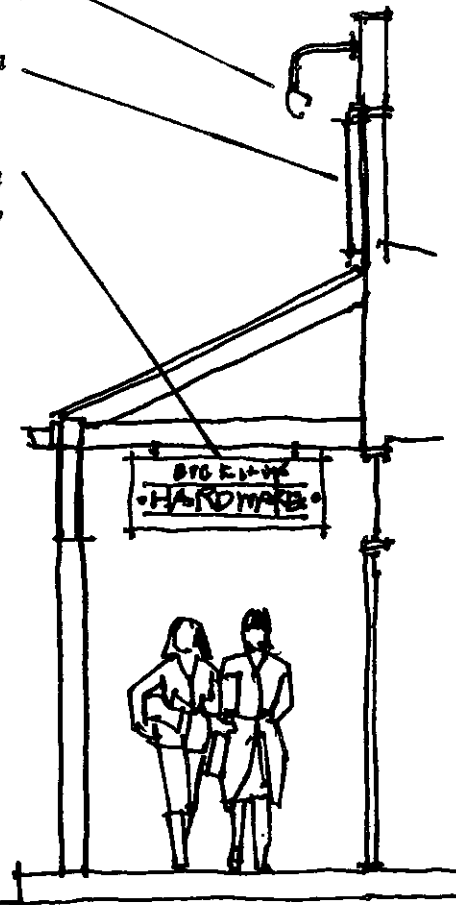


External Lighting

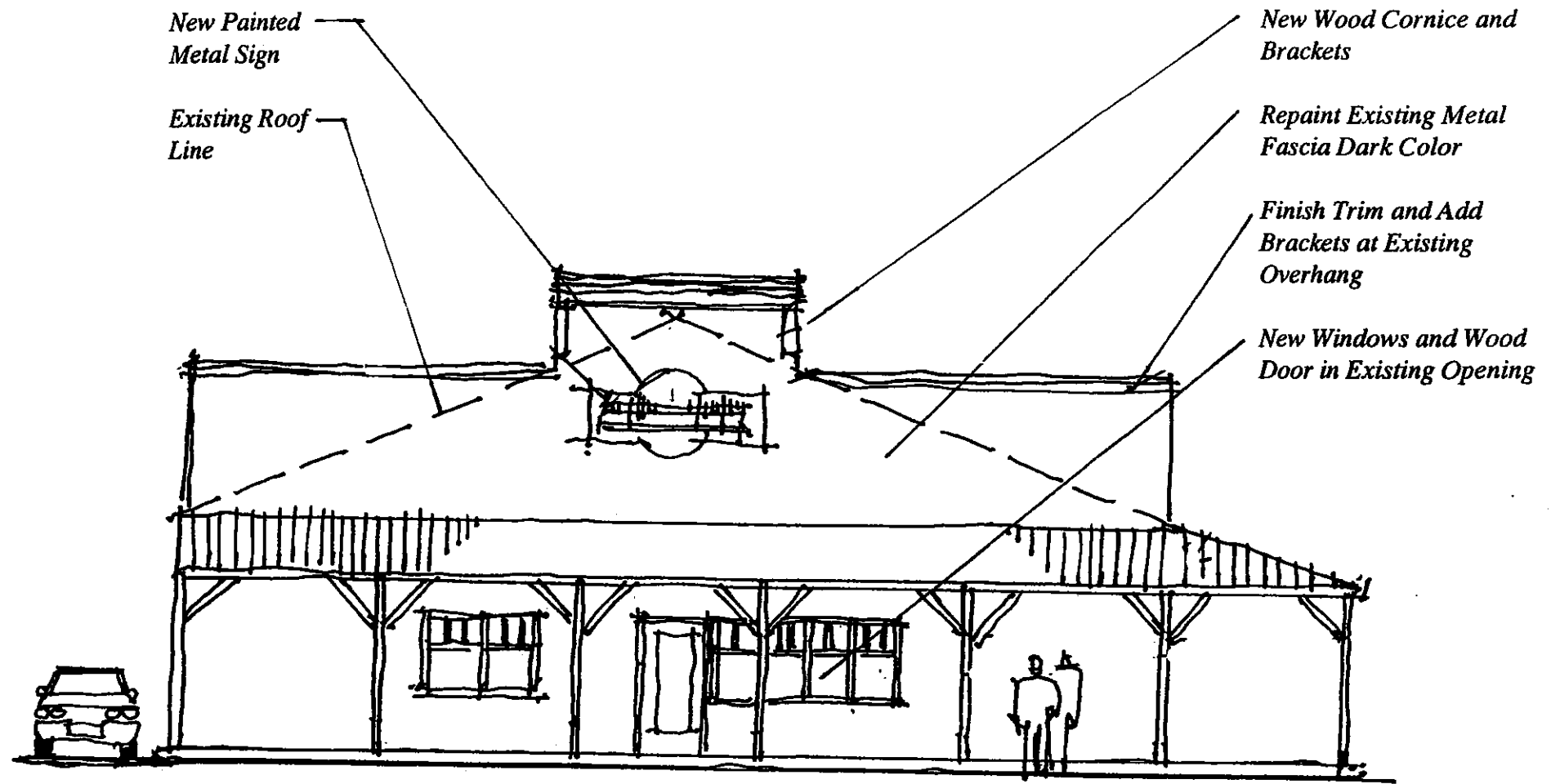
Painted Sign on Wood or Metal

Projecting Sign under Walkway

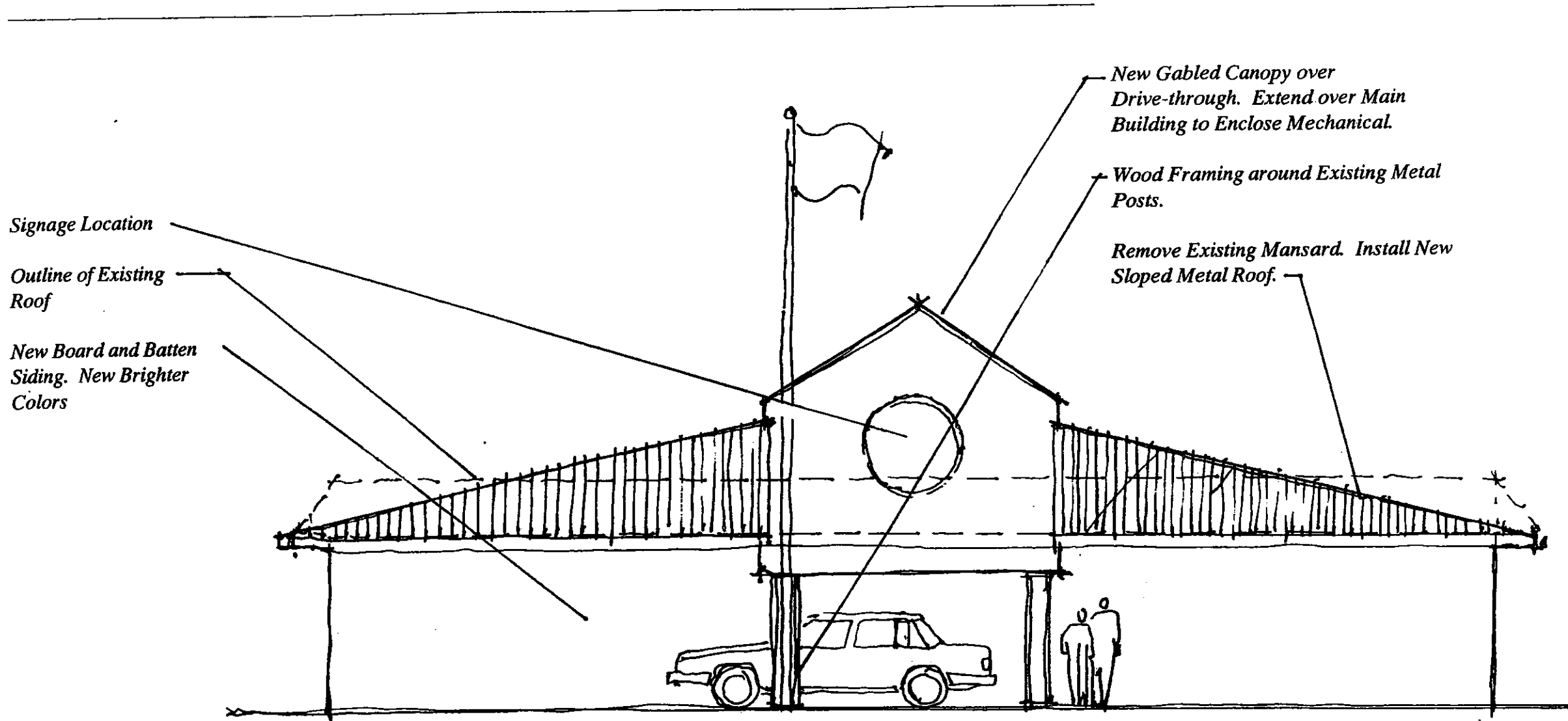
Window Signage



Highway 410 Building Signage



Example: Mr. Auto Renovation



Example: Eagles Lodge Renovation

Copperplate
Gothic
Heavy

ATF

ABCDEFGHIJKLMNOPQRSTUVWXYZ
WXYZ1234567890&\$?!;

E1 | **E1** | **E1** | **E1** | **E1**

21-60-CN | 21-48-CN | 21-36-CN | 21-30-CN | 21-24-CN

E1 | **E1** | **E1**
21-18-CN | 21-14-CN | 21-12-CN

abcdefghijklmnopqrstvwxyz
ABCDEFGHIJKLMNOPQRSTUVWXYZ
1234567890 B &?!.\$\$(,;:)

Playbill
Stephenson Blake

E1 | **a** | **E1** | **a** | **E1** | **a** | **Ea1** | **Ea1** | **Ea1**

60-96-CN | 60-96-L | 60-72-CN | 60-72-L | 60-60-CN | 60-60-L | 60-48-CLN | 60-42-CLN | 60-24-CLN



Examples of Appropriate Signage Lettering Styles

Quentin

ABCDEFGHIJKLMNOPQRSTUVWXYZ
1234567890 &?!@'(.,:;)

E1 E1

117-72-CN

117-48-CN

Rockwell
Condensed 414
Monotype

abcdefghijklmnopqrstuvwxy
ABCDEFGHIJKLMNOPQRSTUVWXYZ
1234567890 æøßÆØ &?!@%(.,:;)

Ea1 Ea1 Ea1

327-48-CLN

327-36-CLN

327-24-CLN

Rockwell Shadow
Monotype

ABCDEFGHIJKLMNOPQRSTUVWXYZ
1234567890 ÆØ &?!@%(.,:;)

E1 E1

411-48-CN

411-36-CN



Examples of Appropriate Signage Lettering Styles